

**Time and Date**

2.00 pm on Thursday, 27th May, 2021

Place

Council Chamber - Council House

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1. **Apologies for Absence**

Additional Document: Late Representation Report

2. **Declarations of Interest**

3. **Members Declarations of Contact on Planning Applications**

Members are reminded that contacts about any planning applications on this agenda must, unless reported to this meeting by the Strategic Lead for Planning, be declared before the application is considered.

4. **Minutes of the meetings held on 8 and 22 April 2021** (Pages 11 - 22)

5. **Late Representations**

To be circulated at the meeting.

6. **Application OUT/2019/0484 - Thompsons Farm, Thompson Road** (Pages 23 - 56)

Report of the Strategic Lead for Planning

7. **Application FUL/2020/2794 - Lyons Park, Former Lawrence Automotive Site, Sayer Drive** (Pages 57 - 94)

Report of the Strategic Lead for Planning

8. **Application FUL/2021/0315 - 97 Broad Lane** (Pages 95 - 108)

- Report of the Strategic Lead for Planning
9. **Application FUL/2021/0369 - Land at Westwood Heath Road, to the rear of St John's Church** (Pages 109 - 120)
Report of the Strategic Lead for Planning
 10. **Application TELO/2021/0198 - Land at Scots Lane** (Pages 121 - 128)
Report of the Strategic Lead for Planning
 11. **Application FUL/2020/1334 - Garages, Overdale Road** (Pages 129 - 152)
Report of the Strategic Lead for Planning
 12. **Outstanding Issues**
There are no outstanding issues.
 13. **Any other items of public business which the Chair decides to take as matters of urgency because of the special circumstances involved**

Julie Newman, Director of Law and Governance, Council House Coventry

Wednesday, 19 May 2021

Note: The person to contact about the agenda and documents for this meeting is Carolyn Sinclair carolyn.sinclair@coventry.gov.uk

Membership: Councillors N Akhtar, P Akhtar, R Auluck (Deputy Chair), R Bailey, L Harvard (Chair), M Heaven, G Lloyd, C Miks, S Nazir, D Skinner and A Tucker

By invitation: Councillor D Welsh

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<https://www.coventry.gov.uk/publicAttendanceMeetings>

Carolyn Sinclair
carolyn.sinclair@coventry.gov.uk

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Late Representations Planning Committee 27 May 2021

<p>Item No. 6</p>	<p>Application No. - OUT/2019/0484</p> <p>Description of Development - Outline permission for the erection of up to 500 residential dwellings with all matters reserved with the exception of access and comprising of: The demolition of Thompsons Cottage and associated buildings; Provision of green infrastructure including strategic open space, sustainable urban drainage, green networks, play space and associated structural and general landscaping; A vehicular access point and emergency access point onto Bennetts Road North; Network of pedestrian and cycle routes; and All associated infrastructure and enabling works</p> <p>Site Address - Land at Thompsons Farm Thompsons Road</p> <p>Introduction The Parish Council's comments were not reported separately within the officer report and further representations have been received.</p> <p>There is an updated position on the NHS contributions.</p> <p>Consultation</p> <p>A response was previously received form Keresley Parish Council who in summary had the following concerns/objections to the scheme:</p> <ul style="list-style-type: none"> • No application should be received until an application has been received for the proposed free school • No planning application should be granted until certainty is provided in respect of delivery of the link road and Sandpits Lane Junction Improvements • The existing bus routes are unsuitable for sustainable transport • There is unsuitable infrastructure provided • The PROW to the north of the site should be enhanced <p>Corley Parish Council have objected raising a new issue of proposed littering.</p> <p>Since writing the report a further 56 objections have been received. In summary, the following additional points have been raised:</p> <ul style="list-style-type: none"> • The proposal is in an unsustainable location • That fly tipping and littering would occur • That the Transport Assessment is unsound • That the proposal would result in flooding • That there are no local job opportunities • That a housing needs assessment should be undertaken to identify a suitable housing mix • That the population statistics are outdated and housing isn't required
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	<p>Appraisal</p> <p>The officer report states that the developer has agreed to all of the requests for s106 contributions. The developer had advised that they (and other developers) in the light of the Secretary of State’s Decision and legal advice, currently have a legal dispute underway with the NHS Trust and at this stage the NHS Trust (Acute Care) contribution is not yet agreed. If the legal dispute finds that the contribution is justified then the developer will pay the requested contributions but they do not want to commit to it until the legal position is known</p> <p>The proposal is considered to be in a sustainable location with suitable and enhanced transport links forming part of the proposal.</p> <p>The Transport Assessment is considered suitable as confirmed by highways who consider that no further highways safety concerns have been presented which would require further consent.</p> <p>The Lead Local Flood Authority have no objections to the proposal on the grounds of flooding subject to conditions.</p> <p>As clarified in the report the recent review undertaken by the Office for National Statistics(ONS) makes no provision or recommendation for altering the existing population numbers.</p> <p>The proposed housing mix will be informed by housing strategy further as part of the S.106 agreement.</p> <p>The alleged absence of local job opportunities isn’t in itself a material consideration.</p> <p>The timescales for delivery of the link road, junction improvements and the school are appropriately controlled by policy and legal agreements separate from this application.</p> <p>The bus routes are being enhanced with a new ‘super stop’ proposed within the site.</p> <p>The Pubic Right of Way to the north of the site is to be retained.</p> <p>The S106 agreement makes suitable provision for infrastructure improvements.</p> <p>Environmental Protection have not raised any concerns in respect of litter/rubbish generation.</p>
<p>Item No. 11</p>	<p>Application No. - FUL/2020/1334</p> <p>Description of Development - Demolition of existing garages and development of 2 x 2-bed bungalows</p> <p>Site Address - Garages Overdale Road</p> <p>Additional representations</p> <p>The following additional comments have been received from Cllr Bally Singh:</p>

I would like to strongly object to this proposal. It will create extra parking strain in the crowded Overdale road area and increase the likelihood of accidents especially near the school - where there have a few near misses and sometimes worse in the area. I have copied in highways who I believe are looking at road safety measures in the area, and I believe this development will be counter to their efforts.

Accessibility will also be impacted negatively for residents, such as for (name of resident removed (part of the row of bungalows.) Residents here rely on the current access for deliveries which have been a life saver during the pandemic - but this development will make deliveries more difficult or unviable. The lack of garage/parking space will also be a significant detriment for local residents.

Third party representations

Three additional representations have been received, one of which is from the headteacher of Whoberley Hall primary school, who has not made comments to date. Concerns due to the potential adverse impact upon traffic levels in the road are raised. The headteacher has been looking closely at the traffic and parking in Overdale Road and Wildcroft Road (other side of the school) with Councillor Becky Gittins (one of the school governors). The comment is made that it is very dangerous at drop off and pick up times for school, and reference is made to a recent incident where a child got hit, luckily, the child was not seriously hurt but the school are very concerned about this. In principle, the changes to the use of the area could benefit school, in that, any anti-social behaviour (possibly drug related) would ease, and we would have more families move into the area (our pupil numbers are low). However, without proper consideration given to the traffic levels this will be a problem for the school's families.

Further comments have been received from two nearby residents. Matters not raised in previous representations include:

- Application is absent of how asbestos is handled during demolition given the associated health risks.
- Proposed plans appear to indicate a possible source of light pollution into rear garden and bedrooms.
- The site has become a blight on the area and residents have been undertaking cleaning and maintenance of the site.
- Citizen Housing's failure to meet their own published policy statement and circumvention of duty of care for residents, the majority of whom have protected characteristics, evidenced by decommissioning and residents ability or otherwise to acquire the garages since July 2018, loss of communal TV reception previously provided by the mast on site, and removal of signage stating that the car park was for residents only.
- The housing is not of benefit to residents or the community at large. The 'Right to Regenerate' would be a simple way to turn public land into public good, with funds available to the community to turn the site into something that benefits the community and residents.

Comments on new matters raised:

Condition 12 requires the submission of a Construction Management Plan to be submitted prior to commencement which includes measures to control the presence of asbestos. Matters relating to residential amenity are addressed in the report. The plans

	<p>do not indicate that there would be a direct light source proposed that would affect neighbours.</p> <p>The Right to Regenerate referred to has been subject to recent consultation by Central Government which closed on 20 March 2021. It is the proposed reform of the Right to Contest, which the consultation says could provide a quicker and easier route for individuals, businesses and organisations to identify, purchase and redevelop underused or empty land in their area, encourage the public to drive regeneration and stimulate the more productive use of public land. The outcome of the consultation has not yet been published.</p> <p>Matters relating to maintenance of the site, removal of signage and loss of television reception from the mast on site are matters separate to the consideration of this application that should be pursued with the applicant, Citizen Housing.</p> <p>Notwithstanding this, it is appreciated that some of the nearby residents are elderly and have mobility problems, and your officer is aware that Citizen have helped/advised garage tenants of other garage sites should they wish to continue to rent a garage. Options available have tried to be kept as local as possible, although alternative garage sites with availability may not be in convenient locations for all residents.</p> <p>Amended Conditions</p> <p>Condition 2- Approved plans and documents</p> <p>For completeness the Highway Statement reference CGSW407-BSP-ZZ-XX-RP-D-0001-P02 and dated 29 April 2020 should be added to the list of approved documents.</p>
<p>Item No. 7</p>	<p>Application No. - FUL/2020/2794</p> <p>Description of Development - Construction of an industrial unit (General Industrial (Class B2) and/or Storage and Distribution (Class B8) use) with ancillary offices, access, servicing, parking, drainage and landscaping. Construction of HGV trailer parking for adjacent unit (Class B8) with pre-check offices, lighting, drainage and landscaping; and all other ancillary works.</p> <p>Site Address - Lyons Park Former Lawrence Automotive Site Sayer Drive</p> <p>Proposal</p> <p>There are a few of minor factual errors in the main report. The number of HGV trailer parking spaces would be 173, as opposed to the 185 referred to in the report. Also, the combined ancillary office / welfare floorspace would be 1,216 sq m as opposed to the 1,257 sq m; and the pre-check offices would have a combined floorspace of 62 sq m compared to the previously stated 13 sq m. None of these changes have any bearing on the officer assessment.</p> <p>The latest unit plan drawing is drawing number 200 P6, as opposed to 200 P5, which is referred to in condition 2 of the schedule. This shall be amended accordingly.</p> <p>Consultation responses</p> <p>One additional objection has been received, expressing concerns with regard to noise and air quality impact. The objector is concerned about individual and cumulative impact and suggest noise and air quality management plans are required.</p>

Appraisal

An additional condition is recommended to ensure no detrimental impact to the occupiers of Highfield House with regard to noise and in accordance with the Noise Assessment submitted.

Additional Condition:

Prior to the use of the hereby approved trailer park within Phase 1, an acoustic barrier shall be provided to the south eastern boundary of the trailer park in accordance with details to be submitted and approved in writing by local planning authority. The barrier shall be maintained and retained thereafter.

To safeguard the amenities of the adjoining occupiers of the development in accordance with Policy DE1 of the Coventry Local Plan 2016.

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Coventry City Council
Minutes of the Meeting of Planning Committee held at 2.00 pm on Thursday, 8
April 2021

Present:

Members: Councillor L Harvard (Chair)
Councillor N Akhtar
Councillor P Akhtar
Councillor A Andrews
Councillor R Auluck
Councillor R Bailey
Councillor L Kelly
Councillor G Lloyd
Councillor C Miks
Councillor D Skinner
Councillor S Walsh

Other Members: Councillor D Welsh, Cabinet Member for Housing and Communities

Employees (by Directorate):

L Albrighton, Highways and Transportation
R Back, Planning and Regulation
S Bennett, Law and Governance
M Bharaj, Planning and Regulation
D Henry, Planning and Regulation
A Lynch, Planning and Regulation
U Patel, Law and Governance
F Runacre, Planning and Regulation
R Thomas, Highways and Transportation
C Thomson, Law and Governance

Apologies: None

Public Business

155. **Declarations of Interest**

There were no declarations of interest.

156. **Members Declarations of Contact on Planning Applications**

The Members named declared contacts on the following application as indicated:

Application No.	Councillor	From
FUL/2020/3127	Councillor Andrews Councillor Harvard Councillor Lloyd	Local residents

157. **Late Representations**

The Committee noted a tabled report which summarised late representations and responses on the following:

Application	Site	Minute
FUL/2020/3127	6-7 Spencer Road	159
ADV/2020/2765	377 Green Lane	161

158. **Application S73/2021/0337 - Land to the North and South of A45**

Application S73/2021/0337 – Land to the North and South of A45, was deferred to a future meeting.

159. **Application FUL/2020/3127 - 6-7 Spencer Road**

The Committee considered a report of the Strategic Lead for Planning, detailing the above application for a proposed change of use of existing student accommodation (sui generis) to supported housing for homeless persons (sui generis), with no physical internal or external alterations to existing building, which was recommended for approval.

A member of the public spoke in respect of his objections to the application. The applicant's representative spoke in support of the application.

RESOLVED that planning permission be granted in respect of Application FUL/2020/3127 subject conditions and the completion of a S106 Agreement to ensure the use remains for the purposes of housing homeless persons.

(Note: Councillor L Kelly did not take part in the decision or voting on this application as she was not present for the whole presentation).

160. **Application FUL/2020/2781 - 300 Kingfield Road**

The Committee considered a report of the Strategic Lead for Planning, detailing the above application for a change of use of premises from Use Class B2 'general industry' to use as a fitness centre falling within Use Class E 'Commercial, Business and Service', which was recommended for refusal.

Councillor B Kaur, a Foleshill Ward Councillor, submitted a statement which was read at the meeting in support of the application. The applicant spoke in support for the application.

Following consideration of the report and matters raised at the meeting, Members considered that the proposed use as a fitness centre was acceptable and were satisfied that access to the premises for pedestrians was safe.

RESOLVED that planning permission be approved in respect of Application FUL/2020/2781 and that Officers be delegated to apply appropriate conditions.

161. Application ADV/2020/2765 - 377 Green Lane

The Committee considered a report of the Strategic Lead for Planning, detailing the above application for a display of new fascia board with illuminated signage to the front and non-illuminated signage to the side, which was recommended for approval.

The Late Representation report indicated that amended plans had been received to show a pitched roof in accordance with the previously approved permission.

A registered speaker sent a statement which was read at the meeting in respect of their objections to the application and the applicant's representative had a statement read at the meeting in support of the application.

RESOLVED that advertisement consent be granted in respect of Application ADV/2020/2765 subject conditions.

162. Outstanding Issues

There were no outstanding issues.

163. Any other items of public business which the Chair decides to take as matters of urgency because of the special circumstances involved

There were no other items of public business.

(Meeting closed at 4.15 pm)

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Coventry City Council
Minutes of the Meeting of Planning Committee held at 2.00 pm on Thursday, 22
April 2021

Present:

Members: Councillor L Harvard (Chair)
Councillor N Akhtar
Councillor P Akhtar
Councillor A Andrews
Councillor R Auluck (Deputy Chair)
Councillor R Bailey
Councillor G Lloyd
Councillor C Miks
Councillor D Skinner
Councillor S Walsh

Other Members: Councillor B Gittins
Councillor G Williams

Employees (by Directorate):

L Albrighton, Highways and Transportation
S Bennett, Law and Governance
L D'Onofrio, Planning and Regulation
R Edgington, Planning and Regulation
S Evans, Law and Governance
R Hall, Highways and Transportation
D Leadon, Planning and Regulation
A Lynch, Planning and Regulation
U Patel, Law and Governance
F Runacre, Planning and Regulation
A Saleem, Planning and Regulation
C Sinclair, Law and Governance
R Thomas, Highways and Transportation
A Walster, Director of Streetscene and Regulatory Services

Apologies: Councillor L Kelly and D Welsh (Cabinet Member for Housing and Communities)

Public Business

164. Declarations of Interest

Councillor A Andrews declared an "Other Relevant Interest" in the items referred to in Minutes 169, 170 and 171 below (Applications OUT/2020/2876 – City Centre South, LB/2020/2858 – Retail Market Queen Victoria Road and LB/2020/2860 – 8A Bull Yard). The interest arose through his role at the West Midlands Combined Authority. He remained in the meeting for the consideration and voting on the applications.

165. **Members Declarations of Contact on Planning Applications**

The Members named declared a contact on the following applications as indicated:

Application No.	Councillor	From
S73/2021/0337 – Land to the North and South of A45	All members of Committee	Objectors
OUT/2020/2876 – City Centre South	All members of Committee	Objectors
LB/2020/2857 – Retail Market Queen Victoria Road	All members of Committee	Objectors
LB/2020/2860 – 8A Bull Yard	All members of Committee	Objectors

166. **Minutes of Previous Meeting held on 25 March 2021**

The minutes of the meeting held on 25 March 2021 were agreed as a true record.

167. **Late Representations**

The Committee noted a tabled report which summarised late representations and responses on the following:

Application	Site	Minute
S73/2021/0337	Land to the North and South of A45	168
OUT/2020/2876	City Centre South	169
LB/2020/2860	8A Bull Yard	171
FUL/2020/2053	Coombe Social Club Brinklow Road	172
FUL/2021/0221	43(Ground Floor) Earlsdon Street	173

168. **Application S73/2021/0337 - Land to the North and South of A45**

The Committee considered a report of the Strategic Lead for Planning detailing the above application for the removal of condition 19 to remove the requirement for layby imposed on planning permission FUL/2019/2671 for construction of grade separated junction with associated access roads, landscaping, drainage and engineering works granted on 25 June 2020.

The proposal relates to previous application FUL/2019/2671 which was granted permission last year for a grade separated junction with associated access roads, landscaping, drainage and engineering works. This permission included the

closure of an existing layby on the westbound carriageway. Condition 19 of that permission requires a suitable replacement layby to be provided and requires an options report to be submitted which identifies a suitable replacement facility. The application now made under Section 73 of the Town and Country Planning Act 1990 (as amended) is for that condition to be removed. If approved there would be no requirement for a replacement layby the application states that this is required as *“following technical work undertaken, and dialogue with the City Council, it has been confirmed that no suitable (safe), deliverable option exists”*.

The late representation document tabled at the meeting appraised a further objection received since the publication of the report.

Councillor G Williams, a Bablake Ward Councillor was present at the meeting and spoke in respect of his objections to the application. A couple of registered speakers had their statement read in respect of their objections to the application.

Following consideration of the report and the representations made at the meeting, the Committee noted and expressed disappointment that a formal response to the consultation had not been submitted by Solihull Metropolitan Borough Council.

RESOLVED that planning permission be granted in respect of Application S73/2021/0337 subject to conditions.

169. **Application OUT/2020/2876 - City Centre South**

The Committee considered reports of the Strategic Lead for Planning and received a comprehensive presentation detailing connected applications for the hybrid City Centre South, the Listed Building Consent for the Retail Market Queen Victoria Road and the Listed Building Consent for 8A Bull Yard. Although the Committee received one overarching presentation, a decision was taken on each application separately. The applications were as per the following:

A: Full application for removal of bridge link between Coventry Market roof top car park and roof top parking over existing retail units on Markey Way and associated reinstatement works to roof top car park surface and balustrade, removal of existing Coventry Markey basement ramp from Rover Road and associated infilling and reinstatement works, works to retaining wall to north-east of Coventry Market, removal of existing pedestrian ramp into Coventry Market off Rover Road, creation of new Coventry Market basement ramp from Queen Victoria Road and associated works to Coventry Market basement, and removal and relocation of William Mitchell mural from front elevation of the former Three Tuns Public House building in the Bull Yard; Outline application:

B: For part of the site (as defined within the Application Boundary Area Plan) for the demolition of all existing buildings and redevelopment of the land for mixed uses, including details of the layout and scale of new development, with details of access, appearance and landscaping reserved; and

C: For part of the site for the demolition of all existing buildings and the redevelopment of the land for mixed use, with details of access, layout, scale, appearance and landscaping reserved. The scheme comprises a mixed use redevelopment of up to 1,300 residential units (Class C3), up to 150 hotel rooms (Class C1), up to 37,500sqm of mixed-use non-residential floorspace including Class E Commercial, Business and Service uses, Class F.1 Learning and Non-

Residential Institutions, and Sui Generis Pub or Drinking Establishment/Hot Food Takeaway/Cinema uses, hard and soft landscaping and new public open spaces including sustainable urban drainage systems, car parking provision and formation of new pedestrian and vehicular access and stopping up of existing highway.

LB/2020/2857 – Listed Building Consent for: Removal of bridge link between Coventry Market roof top car park and roof top parking over existing retail units on Market Way and associated reinstatement works to roof top car park surface and balustrade, removal of existing Coventry Market basement ramp from Rover Road and associated infilling and reinstatement works, works to retaining wall to north-east of Coventry Market, removal of existing pedestrian ramp into Coventry Market off Rover Road, creation of a new Coventry Market basement ramp from Queen Victoria Road and associated works to Coventry Market basement.

LB/2020/2860 – Listed Building Consent for: Removal and relocation of the Grade II listed William Mitchell Mural from the front elevation to a new location to be agreed in writing with Coventry City Council in consultation with Historic England.

The late representations document tabled at the meeting corrected typos within the officer report where listed building consent reference LB/2020/2857 was incorrectly referred to as LB/2020/2875. In addition, the document appraised further representations received and outlined additional and amended conditions.

A registered speaker was present at the meeting and spoke in respect of his objections to the application. Councillor J O’Boyle, Cabinet Member for Jobs and Regeneration, attended the meeting and spoke in support of the application. The Applicant’s representative also spoke in support of the application.

RESOLVED that the grant of planning permission be delegated to the Strategic Lead for Planning in respect of Application OUT/2020/2876 and the associated applications for Listed Building Consent reference LB/2020/2857 and LB/2020/2860, subject to the conditions listed in the report and the late representations document and the completion of a S106 Legal agreement to secure the review of contributions and obligations at subsequent Reserved Matters stage, as identified within the report and subject to the Secretary of State not wishing to intervene regarding determination of the application(s).

170. **Application LB/2020/2857 - Retail Market Queen Victoria Road**

Further to Minute 169 above, the Committee considered a report of the Strategic Lead for Planning detailing the above application for the removal of bridge link between Coventry Market roof top car park and roof top parking over existing retail units on Market Way and associated reinstatement works to roof top car park surface and balustrade, removal of existing Coventry Market basement ramp from Rover Road and associated infilling and reinstatement works, works to retaining wall to north-east of Coventry Market, removal of existing pedestrian ramp into Coventry Market off Rover Road, creation of new Coventry Market basement ramp from Queen Victoria Road and associated works to Coventry Market basement.

RESOLVED that planning permission be granted in respect of application for Listed Building Consent, reference LB/2020/2857 relating to works to

Coventry Market, subject to conditions and the Secretary of State not wishing to intervene regarding determination of the application.

171. Application LB/2020/2860 - 8A Bull Yard

Further to Minute 169 above the Committee considered a report of the Strategic Lead for Planning detailing the above application for Listed Building Consent for the removal and relocation of the Grade II listed William Mitchell Mural from the front elevation to a new location to be agreed in writing with Coventry City Council in consultation with historic England.

The late representations document tabled at the meeting summarised an additional representation received following the publication of the report.

RESOLVED that planning permission be granted in respect of the application for Listed Building Consent, reference LB/2020/2860 relating to the Three Tuns Mural, subject to conditions and the Secretary of State not wishing to intervene regarding determination of the application.

172. Application FUL/2020/2053 - Coombe Social Club Brinklow Road

The Committee considered a report of the Strategic Lead for Planning detailing the above application for the erection of patio/smoking area with new boundary walls and railings, alongside a new draught lobby and landscaping. The application was recommended for refusal.

The late representations document tabled at the meeting reported on an additional representation that had been received following publication of the report and made reference to two petitions that had been received, one in support of the application and one objecting to the application.

A registered speaker attended the meeting and spoke in respect of her objections to the application. The Committee considered the petition in support of the application bearing 618 signatures sponsored by Councillor F Abbott, a Wyken Ward Councillor. Councillor Abbott and the Applicant, who submitted the petition, were present at the meeting and spoke in support of the application.

Following consideration of the report and the representations made at the meeting, the Committee considered that the application would be acceptable with appropriate conditions attached in respect of materials, usage of the external area to be limited to club members and no later than 10.00pm, a management plan detailing how the external area is to be managed and conditions consistent with those recommended by highways.

RESOLVED that planning permission be granted in respect of Application FUL/2020/2053, subject to conditions.

173. Application FUL/2021/0221 - 43(Ground Floor) Earlsdon Street

The Committee considered a report of the Strategic Lead for Planning detailing the above application for the change of use of ground floor from retail (Use Class E) to

restaurant/takeaway (Sui Generis) with extract flue. The application was recommended for refusal.

The Committee considered a petition objecting to the application bearing 72 signatures sponsored by Councillor B Gittins, an Earlsdon Ward Councillor. Councillor Gittins and Councillor G Williams, the petition organiser attended the meeting and spoke in respect of the petition. The Applicant had submitted a statement in support of his application, which was read out on his behalf.

RESOLVED that planning permission be refused in respect of Application FUL/2020/0221.

(Note: Councillor N Akhtar did not vote on this application as he had left the meeting.)

174. Application FUL/2020/2879 - Land Adjacent to Spring Cottage Watery Lane

The Committee considered a report of the Strategic Lead for Planning detailing the above application for the erection of a detached 3 bedroomed bungalow with detached double garage and associated parking and landscaping, following demolition of existing buildings. The application was recommended for refusal.

A registered speaker attended the meeting and spoke in support of the application. The applicant's agent was present at the meeting and also spoke in support of the application.

RESOLVED that planning permission be refused in respect of Application FUL/2020/2879.

(Note: Councillor N Akhtar did not vote on the application as he had left the meeting.)

175. Outstanding Issues

There were no outstanding issues.

176. Any other items of public business which the Chair decides to take as matters of urgency because of the special circumstances involved

1. Further to Minute 137 of the meeting held on 4 March 2021, the Committee received a verbal update in respect of Application OUT/2019/0022, Land at Fivefield Road and Tamworth Road which was approved subject to conditions.

The approval included Condition 17 which stated that "There shall be a buffer of at least 15m from the ancient woodland boundary, where no development is permitted. No buildings shall be permitted within 25m of the ancient boundary".

The Committee were informed that this was superseded by a recommendation from Ecology which stated that there should be a buffer of at least 20m from the ancient woodland boundary where no development is permitted, and no buildings shall be permitted within 30m of the ancient boundary.

RESOLVED that Condition 17 be amended as outlined above.

2. The Chair referred to Councillor A Andrews and Councillor L Kelly who were not standing in the forthcoming Municipal Elections.

The Chair and Members of the Committee paid tribute to the work carried out by the Councillors and wished them well for the future.

(Meeting closed at 6.40 pm)

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Planning Committee Report	
Planning Ref:	OUT/2019/0484
Site:	Thompsons Farm, Thompsons Road
Ward:	Bablake
Proposal:	Outline permission for the erection of up to 500 residential dwellings with all matters reserved with the exception of access and comprising of: The demolition of Thompsons Cottage and associated buildings; Provision of green infrastructure including strategic open space, sustainable urban drainage, green networks, play space and associated structural and general landscaping; A vehicular access point and emergency access point onto Bennetts Road North; Network of pedestrian and cycle routes; and All associated infrastructure and enabling works
Case Officer:	Dean Leadon

SUMMARY

This is an outline application for development of agricultural land allocated for residential development within the Keresley Sustainable Urban Extension (SUE). The proposal is to redevelop the site for up to 500 residential dwellings of mixed size, tenure and type. This application deals with vehicular access/egress for the site and includes demolition of the existing cottage located off Bennetts Road North and associated buildings. The provision of green infrastructure and a network of pedestrian and cycle routes with associated infrastructure also form part of the proposals.

All detailed matters are reserved except for access, which is to be considered in detail as part of this proposal.

The main issues to be considered are the principle of development; impact on the landscape / character of the area; highway considerations; drainage and flood risk; air quality; green infrastructure; and developer contributions.

BACKGROUND

The site was previously designated Green Belt land, however it was removed from the Green Belt following independent examination by an Inspector at Local Plan Examination in Public and allocated for a Sustainable Urban Extension (herein after referred to as the SUE) under Policy DS4 (Part D) of the Coventry Local Plan 2016. The specific housing requirements are allocated under Policy H2:2 of the Coventry Local Plan, 2016.

The application site was allocated for residential development via the Coventry Local Plan adopted in December 2017 as part of the SUE at Keresley. The Keresley SUE was identified to deliver 3,100 new homes

KEY FACTS

Reason for report to committee:	More than 5 representations contrary to recommendation
Current use of site:	Agriculture
Proposed use of site:	Residential

Maximum number of dwellings:	500
Area of Site:	20.7ha
Percentage of site to be developed (houses, gardens and roads:	74%

RECOMMENDATION

Planning committee are recommended to grant planning permission, subject to the conditions listed in the report and the completion of the S106 legal agreement to secure the contributions listed within the report; and

Delegate authority to the Strategic Lead Planning (following consultation with the Chair of Planning Committee) to agree any amendments to these conditions and any S106 triggers/contributions set out within the report, which are considered necessary.

REASON FOR DECISION

- The proposal is consistent with an allocation in an up to date development plan and is acceptable in principle.
- The proposal will not adversely impact upon highway safety, air quality, ancient woodland or ecology.
- The proposal makes provision for necessary developer contributions.
- The proposal accords with Policies DS1, DS3, DS4, H1, H2, H3, H4: H6, H9, GE1, GE3, GE4, DE1, HE2, JE7, HW1, AC1, AC2, AC3, AC4, AC5, EM1, EM2, EM3, EM4, EM5, EM6, EM7 and IM1 of the Coventry Local Plan 2016, together with the aims of the NPPF.

BACKGROUND

APPLICATION PROPOSAL

The application is for up to 500 dwellings with primary vehicular access to the site proposed on the west side Bennetts Road North and a secondary emergency access also proposed further south. The primary access would be located approximately 20m north west of the junction with Grove Lane which is located on the opposite side (west side) of Bennetts Road North. The emergency access would be located approximately 30m south of the junction with Grove Lane. Both of these accesses are directly opposite existing semi-detached housing located on the east side of Bennetts Road North. The primary access would link to a central avenue within the application site (subject to detailed design).

The green infrastructure within the site would be located within various areas. At the northern corner of the site an attenuation basin is proposed which would attach to an existing area of vegetation on the north east corner. To the North West boundary, a green swathe of vegetation would be provided with the existing vegetation being retained and further green landscaping being introduced. This would provide a green buffer between the development and the adjoining playing fields and Lower Rock Farm by further green infrastructure. To the east of the existing pond (to be retained) which sits immediately to the south of the proposed 'central avenue' two further attenuation basins with additional landscaping are proposed. Immediately to the west of the pond is the indicative location of a play area. Further landscaped areas are proposed to the south east and southern corners of the site including a further attenuation basin. All of the landscaping is subject to detailed design although the parameters would be fixed by the relevant parameters plan.

The application proposes a mix of housing types, tenure and size to be clarified at the detailed design stage. The parameter plan submitted limits the housing to 2 storeys in height on the boundaries of the site with the exception of the north east corner near to the proposed access where dwellings would be between 2 and 3 storeys in height. The 'central avenue' would be limited to between 2 and 3 storey height with building heights being 2 to 2.5 storey further within the site.

Of the 20.7ha site 5.4ha is undeveloped which equates to 26% of the site comprising formal and informal open space including sustainable drainage, landscaped buffer, planting and a children's play area.

The proposal is supported by an Environmental Statement, with chapters covering the following issues: socio economic issues, landscape and visual issues, ecology and nature conservation, archaeological and cultural heritage, ground conditions, water resources, transport and access, noise and vibration, and air quality.

SITE DESCRIPTION

The site which sits on the administrative boundary with North Warwickshire is located on the western side of Bennetts Road North and to the North of Thompsons Road. The site is the northern most site within the Keresley SUE. The surrounding area is mixed in nature with pockets of commercial uses within close proximity to the application site. Immediately to the north of the site is a farm and playing fields. Existing housing is located

to the east and to the south of the site on Thompson’s Road. Immediately to the west is existing agricultural woodland.

The site is currently agricultural arable land and gently undulates with the lowest point located towards the centre of the eastern boundary. The site is divided into small fields separated by small hedgerows and related hedgerow trees. On the northern boundary is an existing hedgerow with trees which will be retained. Similarly, the existing hedgerow to the western boundary will be retained. Towards the centre of the site is an existing pond which the applicant intends to retain.

PLANNING HISTORY

There have been a number of historic planning applications on this site; the following are the most recent/relevant:

Application Number	Description of Development	Decision and Date
R/2005.0789	Change of Use from arable land to sports playing field in association with existing rugby football club(Bennetts Road North)	Approved 8 th July 2005
SCO/2018/3007	Scoping Opinion for Proposed development of up to 500 dwellings(Thompsons Road, Thompsons Farm)	Decision 21 st December 2018

POLICY

National Policy Guidance

National Planning Policy Framework (NPPF). The NPPF sets out the Government’s planning policies for England and how these are expected to be applied. It sets out the Government’s requirements for the planning system only to the extent that is relevant, proportionate and necessary to do so. The NPPF increases the focus on achieving high quality design and states that it is “fundamental to what the planning and development process should achieve”.

The National Planning Practice Guidance (NPPG) adds further context to the NPPF and it is intended that the two documents are read together.

Local Policy Guidance

The current local policy is provided within the Coventry Local Plan 2016, which was adopted by Coventry City Council on 6th December 2017. Relevant policy relating to this application is:

- Policy DS1: Overall Development Needs
- Policy DS3: Sustainable Development Policy
- Policy DS4: (Part A) – General Masterplan Principles
- Policy H1: Housing Land Requirements
- Policy H2: Housing Allocations
- Policy H3: Provision of New Housing
- Policy H4: Securing a Mix of Housing
- Policy H6: Affordable Housing

Policy H9: Residential Density
Policy GE1 Green Infrastructure
Policy GE3: Biodiversity, Geological, Landscape and Archaeological Conservation
Policy GE4: Tree Protection
Policy JE7: Accessibility to Employment Opportunities
Policy DE1 Ensuring High Quality Design
Policy HE2: Conservation and Heritage Assets
Policy AC1: Accessible Transport Network
Policy AC2: Road Network
Policy AC3: Demand Management
Policy AC4: Walking and Cycling
Policy AC5: Bus and Rapid Transit
Policy EM1: Planning for Climate Change Adaptation
Policy EM2: Building Standards
Policy EM3 Renewable Energy Generation
Policy EM4 Flood Risk Management
Policy EM5 Sustainable Drainage Systems (SuDS)
Policy EM7 Air Quality
Policy IM1: Developer Contributions for Infrastructure

Supplementary Planning Guidance/ Documents (SPG/ SPD):

SPG Design Guidelines for New Residential Development
SPD Delivering a More Sustainable City
SPD Coventry Connected

CONSULTATION

No Objections received from:

- Sustainability
- Ramblers Association
- Planning Policy
- Natural England
- Housing and Policy
- Environment Agency
- Conservation

No objections subject to conditions/contributions have been received from:

- Highways England
- Coventry City Highways
- West Midlands Fire Service
- Sport England
- Education
- Lead Local Flood Authority
- Warwickshire County Council (Highways)
- Trees
- Sport and Active Recreation
- Skills and Employment
- Parks Service
- North Warwickshire Borough Council

- Nuneaton and Bedworth Borough Council
- NHS (Community Care Group)
- NHS (Acute and Emergency Care)
- Environmental Protection
- Ecology
- Public Right of Way Officer

Objections have been received from:

- Transport for West Midlands

At the time of writing the report comments have not been received from:

- Severn Trent Water
- Coal Authority
- Archaeology

Immediate neighbours and local councillors have been notified; site notices were posted and a press notice was displayed in the Coventry Telegraph on 14th March 2019. Further notification letters were sent out on 22nd October 2019 following receipt of amended plans and additional information.

35 letters of objection have been received, raising the following material planning considerations:

- a) That the proposal would be to the detriment of the adjoining Ancient woodland and Ancient Arden Landscape
- b) That the loss of Greenbelt is not necessary due to the inaccurate population boom
- c) That the proposal would impact on local health
- d) That the proposal would greatly reduce biodiversity
- e) That too much traffic would be created
- f) That Air pollution would be increased to unsuitable levels
- g) Road safety concerns
- h) That the proposal would harm wildlife
- i) Lack of an infrastructure strategy to the detriment of existing infrastructure
- j) That the proposal would result in a loss of privacy
- k) There is an insufficient Buffer from the development to the ancient woodland
- l) The existing Drainage is poor
- m) The design of the proposal is substandard
- n) There is no cycle route proposed
- o) There is no provision for recreation
- p) There would be an increase in crime as a result of the proposal.
- q) There is no greenspace or allotment space.
- r) That the number of houses being built are not required due to external factors such as Brexit

Within the letters received the following non-material planning considerations were raised, these cannot be given due consideration in the planning process:

- s) People will not be able to afford the new houses
- t) The houses should be built on brownfield land
- u) Equestrian access rights are allegedly required for Thompsons Road

Councillor Williams has raised the following concerns:

- That the proposal is unsustainable due to the number of dwellings proposed and the lack of amenities with only one entrance to the site.
- Traffic is already a problem on Bennetts Road North and this will make it worse and if Traffic is heading into Coventry it will have to use the already congested Bennetts Road South.
- There is no provision for a primary school or doctors and no thought has been given to the infrastructure generally required to accommodate the development
- That the T Junction does little to slow traffic down.

A question was also raised as to if the cottage demolished for access was done so unnecessarily.

Councillor Birdi has raised general concerns in respect of highways, the impact on the NHS and housing densities.

APPRAISAL

The main issues in determining this application are: principle of development; landscape / visual amenity; heritage assets; highway considerations; flood risk; air quality; ancient woodland; green infrastructure; and contributions.

Principle of development

The Council is required to make decisions in accordance with the statutory development plan unless material considerations indicate otherwise. (Section 38(6) PCPA 2004 and Section 70(2) TCPA 1990). The statutory development plan is the Coventry Local Plan adopted in December 2017. The National Planning Policy Framework (NPPF) is a key material planning consideration.

The site is allocated for development by Policy H2, as part of the Keresley SUE. The policy states that the allocation will provide: 3100 dwellings; two local centres, a two-form entry primary school and contributions towards a secondary school. Surrounding junction improvements are also required as are retention of relevant ecological features and enhancement of screening to existing residential areas. Furthermore, a distributor link road connecting Long Lane and Winding House Lane is required to be fully operational prior to the full completion of the SUE. The new primary school is located elsewhere within the SUE within the Taylor Wimpey site. The Link Road does not pass through the application site but a S.106 contribution has been made towards its construction. The proposal also makes a planning contribution towards required junction improvements and retains and enhances ecological features as appropriate.

Policy R1 allocates two new local Centres for inclusion within the Keresley SUE. The southern local centre already benefits from outline planning permission and will be provided adjacent to Bennetts Road as part of the land currently being developed by Taylor Wimpey. Prior to the submission of this application, discussions were had regarding whether or not a northern local centre should be provided on this site. After detailed consideration, it was decided by officers that it was not essential to provide a northern local centre within the SUE, due to the presence of some local shops / services within Keresley End village.

Arguments have been made regarding the impact of Brexit in reducing the future demand for housing in the City, thus triggering the need for a Local Plan review. However,

following the logic of this argument would result in all Local Plans in the Country being deemed out of date, as these issues affect the whole country. This cannot be a tenable argument. Whilst the long-term impacts of Brexit will not be known for some time, it is not considered to be reasonable to simply stop development of all major allocated sites across the Country whilst the extent of impact, if any, is considered. Besides, if it is concluded that the housing policies in the Local Plan are out of date, the 'tilted balance' of paragraph 11d of the NPPF would have to be applied, which requires planning permission to be granted unless: policies in the NPPF that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the Policies in the NPPF as a whole.

The principle of the loss of best and most versatile agricultural land was established when the site was allocated for development in the Local Plan.

In conclusion, the principle of development is considered to be acceptable and in accordance with the Council's adopted Local Plan.

Impact on landscape / visual amenity

A landscape and visual appraisal forms part of the Environmental Statement. This identifies the impact upon the surrounding area of the proposal.

Overall, the Proposed Development will result in some limited impacts at a localised level. The scale and form is likely to result in impacts which are limited to the site area and its immediate context, which generally include the settlement edge of Keresley End at Bennetts Road North and Thompsons Road.

A range of landscape and visual receptors have been tested and impacts have been identified for both landscape character and for visual receptors. This includes an iterative process whereby potential impacts have informed the landscape strategy for the site and mitigation has become ingrained in the Proposed Development.

The residual impacts identified as part of this process highlight that the greater degree of impact relates to the site and to a very localised area immediately adjacent to the site; the effect on potential receptor groups in the wider landscape context will be generally very limited.

Overall landscape and visual effects are not considered to be significant and consequently, on balance, the Proposed Development is considered acceptable in landscape and visual terms.

Highway considerations

Policy AC1 'Accessible Transport Network' states that development proposals which are expected to generate additional trips on the transport network should: a) Integrate with existing transport networks including roads, public transport and walking and cycling routes to promote access by a choice of transport modes. b) Consider the transport and accessibility needs of everyone living, working or visiting the city. c) Support the delivery of new and improved high-quality local transport networks which are closely integrated into the built form. d) Actively support the provision and integration of emerging and future intelligent mobility infrastructure.

Policy AC3 of the Local Plan acknowledges that the provision of car parking can influence occurrences of inappropriate on-street parking which can block access routes for emergency, refuse and delivery vehicles, block footways preventing access for pedestrians, reduce visibility at junctions and impact negatively on the street scene. Proposals for the provision of car parking associated with new development will be assessed on the basis of parking standards set out in Appendix 5. The car parking standards also include requirements for the provision of electric car charging and cycle parking infrastructure.

Transport Assessment – Junction Assessments

The application is accompanied by a Transport Assessment (TA) with junction assessments being reviewed against agreed future year scenarios taking into consideration the scenarios of other sites being built and then with the Keresley Link Road. The following junctions are relevant in this regard.

Site Access Junctions

Junction 1 is the Site Access/Bennetts Road North/Grove Lane Roundabout. The proposed access to the site was repositioned in November 2019 following an issue in term of securing the required visibility for highway safety purposes. The resulting priority junction has now been assessed and is considered a suitable access to the site from both a capacity and safety point of view. The site access has been through various amendments which have improved road markings, crossing points and swept paths of vehicles accessing the site. The Highway Authority are now satisfied that the access as designed can accommodate the sorts of vehicles typical of a residential development of this scale.

Off Site Junctions

The impact of the proposed development on the following off-site junctions has been assessed:

Junction 2 B4098 Tamworth Road/Fivefield Road

Junction 3: Bennetts Road/Sandpits Lane

Junction 4: Bennetts Road South/Penny Park Lane

Junction 5: B4098 Tamworth Road/Green Road/The Scotchill/B4098 Keresley Road

Junction 6: Parkville Highway/Parkgate Road/Beake Avenue/Penny Park Lane

Junction 7: Long Lane/Brownhill Green Road/B4076 Coundon Wedge Drive/Wall Hill Road

The flow data has been assessed for all of the off-site junctions and a November 2019 response from PJA shows that these junctions would operate with spare capacity in the 2026 future year scenarios. The Local Highway Authority's assessment of these

proposals concludes that there are no further issues to report in terms of junction capacity.

Highway Safety – The local highway authority is content with the details of the collision data and accept that no highway safety concerns have been presented which would require further assessment.

The Keresley SUE Link Road beyond the application site is being promoted by Coventry City Council on the basis that it is to be funded by developments along or adjacent to its proposed route. It was included in the Local Plan process culminating in publication in December 2017. With this in mind, CCC expects this developer to fund a proportionate part of the Keresley Link Road beyond their site boundary in a similar way to other promoters within the Keresley SUE.

Public Right of Way

The applicant is aware that a claim has been made to add a bridleway to the definitive map along Thompson's Lane and that if this claim is successful then the LHA would expect a financial contribution from the developer to bring the route up to a suitable standard for horse riders, cyclists and pedestrians. Such a contribution will be part of a section 106 legal agreement between the developer and the City Council. The required contribution is included in the highway developer contributions table below.

Residential Travel Plan

A Travel Plan also accompanies the application. This has been prepared in accordance with the guidance set out in the Department for Transport's publications 'Good Practice Guidelines: Delivering Travel Plans through the Planning Process' (April 2009) and 'Making Residential Travel Plans Work: Guidelines for New Development' (September 2005).

The principal objective of a Travel Plan is to reduce the amount of single occupancy car travel to and from a site. This objective can be achieved through a package of measures that seek to:

- Reduce reliance on the car through the reduction in the length and number of motorised journeys, in particular those carried out in single occupancy vehicles.
- Promote the use of alternative means of travel which are more sustainable and environmentally friendly.
- Reduce emissions.

A number of measures have been taken following the submission of the Travel Plan and subsequent discussions with the local highway authority

In terms of the suitability of public transport links, following concerns being raised over the distances to the nearest bus stops, a transport hub, 'super bus stop' within the site has now been proposed and is deemed acceptable. An indicative location has been identified approximately within the centre of the site on the central street loop, which has been marked on the masterplan. It is considered that this location is the most appropriate for the following reasons:

- Being located on the central street loop would allow the bus to turn without the need to provide additional infrastructure. Given that Coventry state that this stop would allow

buses to stand for a short time before continuing back into Coventry, this is not considered to affect existing journey times; and

- The location allows for the majority of residents to be located within an approximate 400m walk distance from the stop.

Whilst the exact details will be determined at the reserved matters stage, it is anticipated that the facility will comprise a sheltered waiting area, real time timetable information and cycle parking as requested by CCC.

Furthermore, the proposed S.106 agreement will make provision for an extended bus route link to the railway stations at Bedworth or Coventry.

Up to 5 years of mobility credits for each household have been agreed these enable users to access a number of modes of alternative transport to the private car including public transport car sharing or green hire schemes.

There are significant improvements and contributions towards cycling contained with the proposed S.106 agreement. This includes formation of relevant sections of a 'superhighway' contributing to links to Coventry City Centre and rail stations and improvements within the immediate vicinity of the site to enable south bound links.

Highway developer contributions

In the interests of clarity, the following CCC highway related developer contributions have been requested and agreed by the applicant:

Keresley SUE Link Road	Contribution towards cost of the link road	£2,568,585.79
Public rights of way	Resurface Thompson's Lane and legal costs for diversion of public footpath	£205,000.00
Traffic Monitoring	Cameras at non-signalised junctions	£7,500.00
Cycling	a) Provide contribution towards cost of cycle superhighway from Long Lane to Holyhead Road	£448,372.63
	b) Provide contribution towards cost of cycle superhighway from Bennetts Road to Barker's Butts Road	£860,898.85
	c) Upgrade of route on western side of site to shared use	£100,000.00
	d) Cycle / eBike / eScooter hire scheme	£20,000.00 TBC

	e) Cycle lane markings & signage on Bennetts Road and Exhall Road	TBC
	f) Road markings, signage and informal crossing of Bennetts Road at jct with Thompsons Road	TBC
Mobility Credits	Up to 5 years of mobility credits for each household	£545,394.80
Public Transport	(a) Demand responsive transport service	£187,720.85
	(b) Extension to the 16 / 16A bus service	£205,573.00
Travel Plan	Travel plan monitoring	£6,800.00
	Residential Travel Plans	£14,125.00
	Household Surveys	£15,039.75
	Travel Plan Coordinator	£32,155.48
	Traffic surveys to determine progress towards mode shift	£20,318.21
Total		£5,237,484.36

Further Highway contributions have been requested by Warwickshire County Council in respect of M6 Junction 3. This is included in the Planning Contributions section of this report with the request made on the basis of the increased traffic impact of the collective development identified with Coventry and Nuneaton and Bedworth's local plans.

In addition, modelling has been undertaken to Winding House Lane/Wheelwright Lane junction and an improvement scheme has been identified. The full costings of this scheme are not yet known and Warwickshire County Council have requested that the scheme be delivered under a section 278 highway agreement. Work is still ongoing to refine the cost and the means of delivery.

Flood Risk

Policy EM4 states that all major developments must be assessed in respect of the level of flood risk from all sources. Policy EM5 states that all development must apply SUDS and encourages discharge to a watercourse if infiltration is not possible.

The site lies wholly within flood zone 1. There are no records held by the Environment Agency or other consulted organisations of flooding occurring within or close to the site. The risk of fluvial flooding to the proposed development will be low. Similarly, the risk of surface water flooding affecting the development proposals is identified as low.

The surface water drainage strategy identifies that some run off will infiltrate into the ground during low intensity rainfall events however due to low permeability more significant rainfall event will result in surface water run-off. Following amendments to the strategy 2 of the 3 remaining outfalls will continue to be the main outfalls post development these are:

- Outfall 2a and 2b – Natural low point within the site, positive outfall to be created into public surface water system with Bennetts Road North
- Outfall 3 – Existing ditch running north-east along Thompsons Lane

A series of attenuation basins are proposed linking to the outfalls included a northern storage basin which will now link by hydroblake flow control to outfall 2a/2b, two new basins linking the existing pond to outfall 2a and 2b and a new southern storage basin connecting to outfall 3.

The Lead Local Flood Authority are satisfied with the surface water drainage proposals subject to a suitably worded condition.

Air quality

Policy EM7 states that major development schemes should promote a shift to the use of sustainable low emission transport to minimise the impact of vehicle emissions on air quality. Development will be located where it is accessible to support the use of public transport, walking and cycling. All major development proposals should be suitably planned to design out any adverse impact on air quality.

The whole of Coventry City falls within an Air Quality Management Area(AQMA).

The submitted Environmental Statement includes an Air Quality chapter, which analyses the impact of the development (including the cumulative impact of the development within the SUE) upon air quality in the area, as well as the air quality within the site.

The Construction Traffic Assessment identified that the number of vehicles a day was estimated to be less than 10 so further assessment of the potential effects on receptors near roads is not required as it is only a requirement in an Air Quality Management Area should the number of vehicles exceed 25 a day. The dust risk identified has been considered during the construction process and it is considered that any potential adverse impacts from dust soiling during construction would be minimised so that residual impacts would not have a significant effect on adjacent human receptors.

The effects on local air quality from changes in road traffic flows within the proposed development within the appropriate year have also been assessed. A number of receptors and road links within the vicinity have been considered. The highest increase in NO₂ concentrations is identified at Parks Leys Medical Practice as this is the closest building on Bennetts Road North between Howat Road and Exhall Road and is expected to attract the largest increase in traffic as a result of the proposed development. This would still fall significantly below the AQS objective of 40micrograms per metre cubed. The magnitude in change in annual mean NO₂ levels as a result of the development is expected to be negligible at all of the receptor locations considered.

Environmental Protection have no objections to the proposal on air quality grounds. They have requested conditions in respect of a Construction Environmental Management Plan, low NO_x boilers and EV Charging points.

Ancient Woodland

Policy GE3 of the Local Plan states that Sites of Special Scientific Interest (SSSIs), Local Nature Reserves (LNRs), Ancient Woodlands, Local Wildlife and Geological Sites will be protected and enhanced.

Paragraph 175 of the NPPF states that “development resulting in the loss or deterioration of irreplaceable habitats, such as ancient woodland and ancient or veteran trees, should be refused unless there are wholly exceptional reasons”.

This proposed development site lies within a valley within the Ancient Arden area of Coventry and was previously been managed as agricultural arable land. It is located north of an area of designated woodland (Bunsons Wood)

The Forestry Commission and Natural England have produced standing advice to assist planning authorities in determining the impact upon ancient woodlands. It states that:

Direct impacts of development on ancient woodland or ancient and veteran trees include: damaging or destroying all or part of them (including their soils, ground flora or fungi)

- damaging roots and understorey (all the vegetation under the taller trees)
- damaging or compacting soil around the tree roots
- polluting the ground around them
- changing the water table or drainage of woodland or individual trees
- damaging archaeological features or heritage assets

Nearby development can also have an indirect impact on ancient woodland or ancient and veteran trees and the species they support. These can include:

- breaking up or destroying connections between woodlands and ancient or veteran trees
- reducing the amount of semi-natural habitats next to ancient woodland
- increasing the amount of pollution, including dust
- increasing disturbance to wildlife from additional traffic and visitors
- increasing light or air pollution
- increasing damaging activities like fly-tipping and the impact of domestic pets
- changing the landscape character of the area

Mitigation measures will depend on the development but could include:

- improving the condition of the woodland
- putting up screening barriers to protect woodland or ancient and veteran trees from dust and pollution
- noise or light reduction measures
- protecting ancient and veteran trees by designing open space around them
- identifying and protecting trees that could become ancient and veteran trees in the future
- rerouting footpaths
- removing invasive species
- buffer zones

It goes on to state that “A buffer zone’s purpose is to protect ancient woodland and individual ancient or veteran trees. The size and type of buffer zone should vary depending on the scale, type and impact of the development. For ancient woodlands, you should have a buffer zone of at least 15 metres to avoid root damage. Where assessment shows other impacts are likely to extend beyond this distance, you’re likely to need a larger buffer zone. For example, the effect of air pollution from development that results in a significant increase in traffic.

Where possible, a buffer zone should contribute to wider ecological networks and be part of the green infrastructure of the area. It should consist of semi-natural habitats such as woodland and / or a mix of scrub, grassland, heathland and wetland planting. You should plant buffer zones with local and appropriate native species. You should consider if access is appropriate and can allow access to buffer zones if the habitat is not harmed by trampling. You should avoid including gardens and drainage systems in buffer zones”.

Following a request from the City Council’s Arboricultural Officer, the proposals have been revised to increase the separation of the proposed development from the offsite area of designated ancient woodland (Bunsons Wood). The closest development parcel is now c.40m distant, which once designed in detail, is anticipated to result in built forms being c.50m from the woodland. This is considered to be a suitable buffer zone to ensure the protection of the woodland from the proposed development.

Green Infrastructure

Trees

A revised comprehensive Arboricultural survey has been submitted with the application and all trees are either earmarked for retention or removal. The survey identifies a requirement to remove five individual trees and the partial removal of two groups of trees and 7 hedgerows. The removal of three category A English Oaks is unavoidable due to the location of access to the site.

The tree officer, raised no objections to the locations of access and general indicative layout, subject to;

- Retention of A cat Oaks T3, 14, 15, plus T43-T47 which are located within Group 19.
- Maintaining a 50m buffer from adjacent ASNW Bunson’s- this would require a 20m wide strip of Green Infrastructure within the site, located south from the Thompson’s Lane boundary.
- A minimum 10m wide buffer strip of Green Infrastructure to the site side (south) tree groups G19 and G20.
- A plan to show restocking of gaps to existing hedgerows, with local species selected from local Ancient Arden guidelines, sourced from local provenance.
- Full details within Arboricultural Method Statement of any incursions within Root Protection Areas.

The revised Arboricultural survey responds to these comments.

Conditions have also been recommended for the applicant to provide a dimensioned Tree Protection Plan and an Arboricultural Method Statement.

Biodiversity

A Biodiversity Impact Assessment has been submitted with the application. Based on the current indicative masterplan and associated open space information the proposed development would result in the loss of approximately 2.52 units representing only 6.2% of the existing habitat.

The biodiversity loss is compensated against by a contribution via the S.106 agreement as detailed in the Developer Contributions section of this report.

Protected species

There is no evidence on site of bat roosting although there are a small number of trees with bat roosting potential which are to be retained as part of the proposal.

No known Badger Setts have been recorded within the site itself although they are known to be within the local vicinity of the site. There is evidence of individuals frequenting the site for foraging although this is limited due to the arable nature of the site. Such opportunities will exist within the new landscaped areas. The effect of the development are therefore considered to be 'non-significant'.

Based on the surveys undertaken it unlikely that a Great Crested Newt will be present within the site however under the proposal the majority of the boundary vegetation (including trees and hedgerows) along with pond P1 and the woodland areas at the site will be retained and as such there is suitable habitat for any future migration.

Open space / play areas

At approximately 26% the development achieves in excess of the minimum 20% open space requirement stipulated by the commentary to Policy H9.

Within the site there is 2.38ha of Parks and Open Space, 1.51ha of natural greenspace and also a formal play space area of 0.12ha. This complies with the requirements of the parks team. The proposed play area provision on site will be larger than a Local Equipped Area for Play (LEAP) but smaller than a Neighbourhood Equipped Area for Play (NEAP) and therefore a planning contribution has been requested and is agreed for BMX/Skate provision and/or green gym/Path at Keresley End Recreation ground.

Developer Contributions

Policy IM1 'Developer Contributions for Infrastructure' states that development will be expected to provide, or contribute towards provision of: a) Measures to directly mitigate its impact and make it acceptable in planning terms; and b) Physical, social and green infrastructure to support the needs associated with the development.

The development would trigger the need for the following contributions to be secured under a Section 106 Legal Agreement. The heads of terms are as follows:

Nuneaton and Bedworth Borough Council Parks Team	BMX / skate provision and/or green gym / path at Keresley End Rec	£59,000
Warwickshire County Highways	Winding House Lane / Wheelwright Lane junction	To deal with by condition or financial contribution as described in Highways section of the report
Warwickshire County Highways		£366, 500 for improving M6 Junction 3
Education (CCC)	Primary school (on wider SUE site) Early Years / Pre-school within 2 miles	£1,182,000
	Primary SEN at Sherbourne Fields	£56,916
	Secondary at PK or Coundon Court	£1,918, 466
	Post 16 at PK or Coundon Court	£379,057
	Secondary and Post 16 SEN at Sherbourne Fields	£86,156
NHS (CCG)	Primary care and healthcare estate within 3 miles of SUE	£266,687.45
NHS (hospital)	Acute and emergency care	£256,027
Housing Policy	Affordable Housing	25%
Sport Team (CCC)	Sports hall / swimming pools at public leisure development in NW of City	£457,434 Inc. Pools £233,737 Sports Hall £223,697
	Playing pitches (improvement / provision / maintenance)	£316,046 (Inc. grass playing pitches, 10-year maintenance and contribution towards an artificial grass pitch)
	Changing room facility	£255,000
Highways (CCC)	Various as listed in Highways section of report	£5,237,484,.36
Ecology (BIA)		Contribution of £101,287 to Coventry City Council would offset the loss of 2.52 units shown in the Warwickshire BIA

The contributions are considered to be compliant with the Community Infrastructure Levy (CIL) Regulations and the developer has agreed to the requested contributions.

Other Matters

The Office for National Statistics(ONS) has recently undertaken a review in terms of the population projections for Coventry. The review found that to ensure future population statistics are based on sound methods and suitable data, ONS's population projections team needs to amongst other procedural changes: *'Investigate the root and scale of the issue associated with cities with large student populations and communicate its findings publicly, to support the appropriate use of the existing data'*. Critically the review makes no provision or recommendation for altering the existing population numbers so there are no direct or immediate implications for Coventry's Local Plan or housing figures.

Given the agricultural history of the site, land contamination reports will be required to be submitted by condition, to protect the amenity of future occupiers.

Similarly, noise reports will be required to be submitted with reserved matters applications in order to identify any mitigation / specific ventilation / glazing specification measures when detailed layouts are known. The main noise source is road traffic.

A Heritage Statement was submitted as part of the Environmental Statement which concludes that there is no adverse harm to the significance of any designated heritage assets. The Conservation officer raises no objection to the application.

An Archaeological Evaluation of the development land was undertaken and forms part of the Environmental Impact Assessment. This included the digging of 34 Trenches each measuring 30 metres long. Archaeological remains were very sparse and comprised two charcoal rich pits located in the centre of site. A number of agricultural features were spread across the area and the majority of features investigated were modern with several identified during the geophysics found to be field drains. A large modern borrow pit was found in one trench and a furrow was found in another containing an abraded fragment of medieval pottery. Post-medieval and modern pottery was found in the topsoil across the site and the southwestern field was also found to contain a fragment of medieval pottery. The majority of trenches were negative for features and therefore no further investigation is required.

Aside from the impacts of construction upon neighbouring residents, which will be partially mitigated by a construction method statement or management plan, the more refined impacts upon residential amenity (such as privacy, outlook and overlooking) will be dealt with through the reserved matters process when detailed layouts are submitted for consideration.

Equality Implications

Section 149 of the Equality Act 2010 created the public sector equality duty. Section 149 states:-

- (1) A public authority must, in the exercise of its functions, have due regard to the need to:
 - a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;

- b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

Officers have taken this into account and given due regard to this statutory duty, and the matters specified in Section 149 of the Equality Act 2010 in the determination of this application. Furthermore, attention will be paid to the duty as reserved matters applications are submitted for the detailed design of buildings and the internal layout of the various development parcels.

Conclusion

The Planning System is plan led. The Local Plan is up to date in policy terms being consistent with the NPPF. It was adopted in December 2017 after extensive publicity, consultation and independent scrutiny. The application site is part of a SUE that through the plan making process was removed from the Green Belt and specifically allocated for development. The consideration of retaining the land as open and undeveloped and the assessment of whether the land comprised of the SUE allocation should be developed was tested at the Examination in Public of the Local Plan by an independent planning Inspector. The development proposed in this application is consistent with the strategy of the Local Plan and accords with the provisions of the adopted Local Plan. The test is that the determination of the application should accord with the development plan unless material considerations indicate otherwise. Determining the application in accordance with the Local Plan would mean approval of the application unless other material considerations were such as to indicate that it should be refused. Having considered the matters raised in the course of the application and consultations summarised in this report it is the view of the officers that no other material considerations are identified that are sufficient to outweigh the presumption in favour of an up to date development plan. The proposed development is considered to be acceptable in principle and will not result in any significant impact upon flooding, heritage assets, character of the area, air quality, highway safety, ancient woodland, ecology or infrastructure, subject to relevant conditions and Section 106 contributions. The reason for Coventry City Council granting planning permission is because the development is in accordance with: Policies DS1, DS3, DS4, H1, H2, H3, H4, H6, H9, HW1, GE3, GE4, DE1, HE2, AC1, AC2, AC3, AC4, AC5, EM1, EM2, EM3, EM4, EM5, EM6, EM7, JE7 and IM1 of the Coventry Local Plan 2016, together with the aims of the NPPF

In reaching this recommendation, the Case Officer has taken into account the ES which was submitted under the Town and Country Planning (Environmental Impact Assessment) (England and Wales) Regulations. Your officer considers that the ES and the further information provided complies with the above regulations and that sufficient information has been provided to assess the environmental impact of the proposals.

CONDITIONS:/REASON

1. Approval of the details of the appearance, layout, scale and landscaping (hereinafter called "the reserved matters") shall be obtained from the local planning authority in writing before each phase of development is commenced and the development within that phase shall be carried out in full accordance with those reserved matters as approved.

Reason: *To conform with Article 5(1) of the Town and Country Planning (General Development Procedure) Order 2015)*

2. Application for approval of the reserved matters for the first phase approved pursuant to condition 5 shall be made to the Local Planning Authority before the expiration of 3 years from the date of this permission. Application for approval of all of the reserved matters shall be made to the local planning not later than 7 years from the date of this permission.

Reason: *To conform with Section 92 of the Town and Country Planning Act 1990 (as amended)*

3. The development of each phase hereby permitted shall take place within two years from the date of approval of the last of the reserved matters to be approved for the relevant phase.

Reason: *To conform with Section 92 of the Town and Country Planning Act 1990 (as amended)*

4. The reserved matters shall be carried out in accordance with the following approved plans: Access and Movement Parameters Plan P16-0926_10-01D; Land Use Parameter Plan P16 -0926_10-03D; Building Heights Parameters Plan P16 - 0926_10-02B, Site Location Plan P16-0926_05E; General Arrangement Site Access Drawing 02427-A-004-P0 and shall have full regard to the Sustainable Urban Extension Design Guidance SPD and include the following specific requirements:
 - (i) provision of a childrens equipped play area;
 - (ii) retention of trees, tree groups and hedgerows indicated for retention in the submitted Arboricultural Impact Assessment Sep 2019;
 - (iii) Inclusion of car club spaces for the SUE wide car club;
 - (iv) Provision of a 'super bus stop' within the site;
 - (v) Provision of cycle hire storage facility.

Reason: *For the avoidance of doubt and in the interests of achieving sustainable development, having particular regard to the potential impact of the development in accordance with Policy DS3 of the Coventry Local Plan 2016.*

5. Prior to the commencement of the development hereby permitted, a site wide phasing programme shall be submitted to and approved in writing by the Local Planning Authority, which shall include:
 - Details of the precise location and extent of individual development phases;
 - The extent of development within each phase and a description of the intended timing of development and completion of each phase;
 - Permanent and temporary access arrangement to serve each phase of development;
 - Any interim surface or boundary details relating to each phase of development;
 - Timings of provision of Green Infrastructure and footpath / cycle routes within each phase;The development shall only be carried out in full accordance with the approved site wide phasing programme.

Reason: *To ensure that in the event of the development being carried out on a phased basis, satisfactory access and interim environmental treatment is provided to address the impact in the interests of public safety and amenity in accordance with Policies DS4, DE1, AC1 and AC2 of the Coventry Local Plan 2016*

6. Any gas boilers installed on site shall have a dry NO_x emission rate of no more than 40mg/kWh. One electric vehicle recharging point per dwelling shall be provided prior to occupation and shall not be removed or altered in any way and shall be kept available for such use by residents at all times.

Reason: *To mitigate the impacts of development on air quality in accordance with Policy DS3 of the Coventry Local Plan 2016 and the aims and the objectives of the NPPF.*

7. An investigation and risk assessment, in addition to any assessment provided with the planning application, must be completed in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The contents of the scheme are subject to the approval in writing of the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The written report is subject to the approval in writing of the Local Planning Authority. The report of the findings must include:
 - (i) a survey of the extent, scale and nature of contamination;
 - (ii) an assessment of the potential risks to:
 - human health,
 - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - adjoining land,
 - groundwaters and surface waters,
 - ecological systems,
 - archaeological sites and ancient monuments;
 - (iii) an appraisal of remedial options, and proposal of the preferred option(s).

Reason: *This must be conducted in accordance with DEFRA and the Environment Agency's 'Model Procedures for the Management of Land Contamination, CLR 11'*

8. Development within any phase shall only be undertaken in accordance with a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment, which shall be submitted to and approved in writing by the local planning authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

9. The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of development within that phase, other than that

required to carry out the remediation. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

10. Prior to occupation of the development within that phase, and following completion of the measures identified within the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be produced and submitted to the local planning authority for approval.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

11. In the event that contamination is found at any time when carrying out the approved development, that was not previously identified, it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of condition No.9, which shall be submitted to and approved in writing by the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with condition No.10.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

12. Noise assessments shall be submitted with reserved matters applications for each phase of development. The assessments will demonstrate by calculation that internal noise levels for the proposed residential property meet the 'Good' criteria set out in British Standard 8233 'Sound Insulation and Noise Reduction for Buildings' together with any mitigation measures that are required to achieve this. The report shall also demonstrate that outdoor garden and leisure areas associated with this development meet the 55dB limit as required by the World Health Organisation (WHO). Prior to the first occupation of the buildings any necessary mitigation measures shall have been implemented in full accordance with the recommendations of the noise assessment and thereafter shall not be removed or altered in any way.

Reason: *To protect the amenities of future occupiers from road traffic noise and general disturbance in accordance with Policy H3 of the Coventry Local Plan 2016.*

13. No development (including any demolition) shall take place within any phase unless and until a Construction Management Plan (CMP) has been submitted to and approved in writing by the Local Planning Authority for that phase of development. The CMP shall include details of:
 - hours of work;
 - hours of deliveries to the site;
 - the parking of vehicles of site operatives and visitors during the demolition/construction phase;

- the delivery access point and routes for HGV's to access the site from the West Midlands Key Route Network;
- the loading and unloading of plant and materials;
- anticipated size and frequency of vehicles moving to/from the site;
- the storage of plant and materials used in constructing the development;
- the erection and maintenance of a security hoarding including decorative displays and facilities for public viewing where appropriate;
- wheel washing facilities and other measures to ensure that any vehicle, plant or equipment leaving the application site does not carry mud or deposit other materials onto the public highway;
- measures to control the emission of dust and dirt during demolition and construction;
- measures to control the presence of asbestos;
- measures to minimise noise disturbance to neighbouring properties during demolition and construction;
- details of any piling together with details of how any associated vibration will be monitored and controlled; and
- a scheme for recycling / disposing of waste resulting from demolition and construction works.

Thereafter, the approved details within the CMP shall be strictly adhered to throughout the construction period and shall not be amended in any way.

Reason: *The agreement of a Construction Management Plan prior to the commencement of development is fundamental to ensure a satisfactory level of environmental protection; to minimise disturbance to local residents and in the interests of highway safety during the construction process in accordance with Policies [EM7], AC1 and AC2 of the Coventry Local Plan 2016.*

14. No development (including any demolition or preparatory works) within any phase shall commence unless and until a Construction Environmental Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority for that phase of development. The CEMP shall include the following:
- (a) a risk assessment of potentially damaging construction activities;
 - (b) identification of biodiversity protection zones (e.g. buffers to trees and hedges or to protected wildlife habitat);
 - (c) practical measures (both physical measures and sensitive working practices, such as protective fencing, exclusion barriers and warning signs) to avoid or reduce impacts during construction (particularly in relation to works within canopy and root protection areas for hedgerows or protected trees);
 - (d) the location and timing of sensitive works to avoid harm to biodiversity features (in relation to breeding birds in particular);
 - (e) the times during construction when specialist ecologists need to be present on site to oversee works (as required);
 - (f) responsible persons and lines of communication; and
 - (g) the role and responsibilities on site of an Ecological Clerk of Works (ECoW) or similarly competent person (as necessary).

The approved CEMP shall be adhered to and implemented throughout the construction period strictly in accordance with the approved details.

Reason: *In order to safeguard protected and/or priority species from undue disturbance and impacts, noting that initial preparatory works could have unacceptable impacts; and in order to secure an overall biodiversity gain in accordance with Policy GE3 of the Coventry Local Plan 2016.*

15. Prior to the first occupation of the development within any phase hereby permitted a Landscape and Ecological Management Plan (LEMP) shall be submitted to and approved in writing by the Local Planning Authority. The content of the LEMP shall include the following:
- a) Description and evaluation of features to be managed;
 - b) Ecological trends and constraints on site that might influence management;
 - c) Aims and objectives of management, including mitigation and enhancement for species identified on site;
 - d) Appropriate management option for achieving aims and objectives;
 - e) Prescriptions for management actions;
 - f) Preparation of a work schedule (including an annual work plan capable of being rolled forward over a ten-year period);
 - g) Details of the body or organisation responsible for implementation of the plan, along with funding mechanism(s) for that body or organisation;
 - h) Ongoing monitoring and remedial measures, including where monitoring shows that conservation aims and objectives of the LEMP are not being met.
- The LEMP plan shall be implemented in strict accordance with the approved details within three months of the first occupation of the development and thereafter shall not be withdrawn or amended in any way.

Reason: *In order to safeguard and enhance habitat on or adjacent to the site in order to secure an overall biodiversity gain in accordance with Policy GE3 of the Coventry Local Plan 2016*

16. Prior to the installation of any street lighting or any external lighting to be fixed to any building within each phase, an external lighting strategy (including a plan) shall be submitted to and approved in writing by the Local Planning Authority. The strategy shall demonstrate that lighting shall be kept to a minimum at night in order to minimise impact on emerging and foraging bats, and to restrict light spillage onto foraging corridors and the ancient woodland. The lighting shall be installed in full accordance with the approved strategy and all lighting thereafter shall be subsequently maintained in strict accordance with the approved details.

Reason: *To ensure that protected species are not harmed by the development in accordance with Policy GE3 of the Coventry Local Plan 2016 and the advice contained within the NPPF 2018.*

17. The following shall be submitted to the Local Planning Authority together with reserved matters applications for each phase of development:
- a) Arboricultural Impact Assessment (5.4) to assess the direct and indirect implications of trees upon the proposal and visa-versa, including locations for underground/ over-ground services, level changes within RPA's etc.;
 - b) Arboricultural Method Statement (6.1); and
 - c) a Dimensioned Tree Protection Plan (to include protection measures during and after construction and any construction exclusion zones) (in accordance with 5.5/

Table B.1), site monitoring (6.3) of British Standard BS5837:2012 - Trees in relation to design demolition and construction - Recommendations, which shall also include any proposal for pruning or other preventative works.

The approved mitigation and / or protection measures shall be put into place prior to the commencement of any works and shall remain in place during all construction work.

Reason: *To protect those trees which are of significant amenity value to the area and which would provide an enhanced standard of appearance to the development in accordance with Policy GE3, GE4 and HE2 of the Coventry Local Plan 2016.*

18. None of the dwellings hereby permitted shall be occupied unless and until the car parking provision for that dwelling has been constructed or laid out, and made available for use by the occupants and / or visitors to the dwellings and thereafter those spaces shall be retained for parking purposes at all time and shall not be removed or altered in any way.

Reason: *To ensure the satisfactory provision of off-street vehicle parking facilities in accordance with the Council's standards and in the interests of highway safety and the satisfactory development of the site in accordance with Policies AC1, AC2 and AC3 of the Coventry Local Plan 2016.*

19. The development hereby permitted shall only proceed in strict accordance with a scheme for targeting and utilising local people for construction and employment, which shall be submitted to an approved in writing by the Local Planning Authority.

Reason: *To secure local employment in accordance with the City Council jobs strategy and Policy JE7 of the Coventry Local Plan 2016.*

20. No development shall commence within a phase unless and until a Sustainable Building Statement has been submitted to and approved in writing by the Local Planning Authority. The statement shall demonstrate how the requirements of Local Plan Policy EM2 (Building Standards) have been met. The development within that phase shall not be occupied unless and until all the works within the approved scheme have been completed in strict accordance with the approved details and thereafter the works shall be retained at all times and shall not be removed or altered in any way.

Reason: *To comply with the provisions of the NPPF and in accordance with Policy EM2 of the Coventry Local Plan 2016.*

21. Notwithstanding the Flood Risk Assessment and Drainage Strategy, the following shall be submitted to the local planning authority together with each reserved matters application:

I. A scheme for the provision of surface water drainage, incorporating SuDS attenuation techniques. There must be consideration of features such as green roofs, rain gardens and swales, for the management of surface water peak and total flows, biodiversity and water filtering, in accordance with Coventry City Council's

adopted Supplementary Planning Document for 'Delivering a More Sustainable City'.

II. A detailed strategy for the long-term maintenance of the SuDS and other surface water drainage systems on site.

III. Development discharge rates to be managed to Qbar greenfield rates minus 20%. The discharge rates for brownfield sites shall be considered as greenfield in accordance with the SFRA.

IV. Evidence the 1 in 100 year plus 40% climate change events will be held within the site boundaries.

V. Provisions must be made for the drainage of the site to ensure there are no temporary increases in flood risk, on or off site, during the construction phase.

VI. Provisions must be made for the drainage of the site to ensure there is no discharge of surface water to the Public Highway.

VII. All proposed outfalls must be confirmed and agreed with the Lead Local Flood Authority prior to the commencement of work on site.

VIII. Evidence that receiving water bodies or sewers are capable of accepting the attenuated flows specified by the Lead Local Flood Authority and that this will not exacerbate the flood risk on or off site. This will include capacity calculations and outcomes, not just the correspondence from Severn Trent Water Ltd in isolation, accepting the point discharges. Evidence of existing sub catchments within the site are needed to support the connectivity survey and confirm the acceptability of proposed point discharges to the watercourses and infrastructure sewers. This must be submitted to, and agreed by, the Local Planning Authority and Lead Local Flood Authority

IX. Single discharge points will be discouraged on larger sites, as discharge points are to be located to best mimic the natural discharge condition.

X. A minimum 5m way leave must be provided from the top bank of any ordinary watercourse and open water bodies.

XI. The development must be considered for the implementation of permeable paving or similar permeable material for the management of total surface water flows, and water filtering in accordance with Coventry City Council's adopted Supplementary Planning Document for 'Delivering a More Sustainable City'.

XII. Evidence to show the management of overland flow routes in the event of exceedance or blockage to the drainage system. Details should include demonstration of how the building will be protected in such an event. Finished floor slab levels must be 300mm above the 1 in 100 year pluvial flood levels.

XIII. Where new or redevelopment site levels result in the severance, diversion or the reception of natural land drainage flow, the developer shall maintain existing flow routes (where there are no flood risk or safety implications) or intercept these flows and discharge these by a method approved by the Local Planning Authority

XIV. The drainage strategy should not result in top water levels of attenuation structures being above the natural ground level and must achieve a 300mm freeboard, in relation to this existing ground level, at the 1 in 100 year plus climate change event.

XV. Where an attenuation structure is located adjacent to Public Highway boundary, the applicant should demonstrate the structural adequacy of the attenuation structure to safeguard Public Highway.

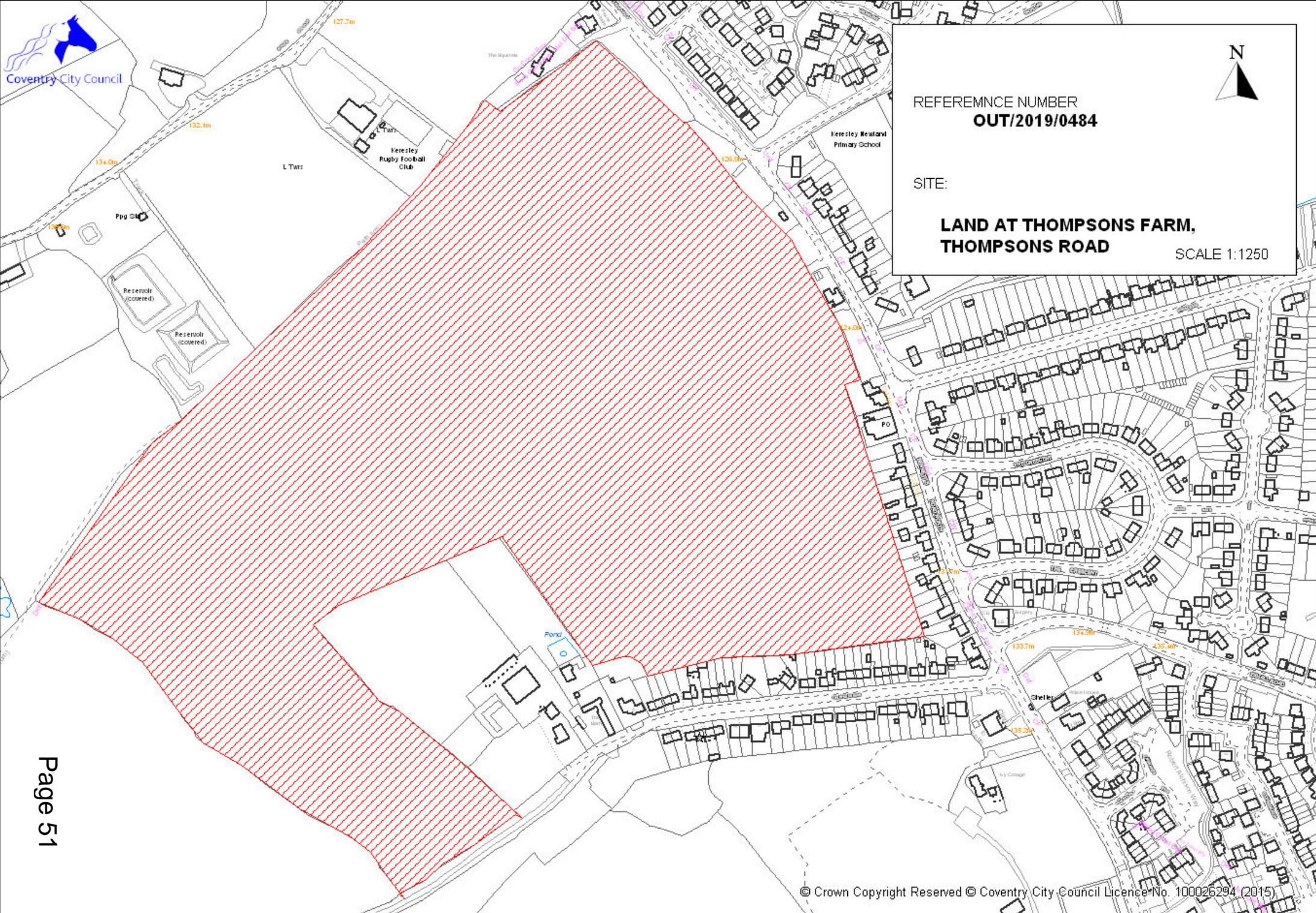
XVI. Foul drainage plans

Reason: *To ensure that adequate drainage facilities are available for the satisfactory and proper development of the site in accordance with policies EM1, EM4 and EM5 of the Coventry Local Plan 2016 and Coventry City Council's adopted Supplementary Planning Document for 'Delivering a More Sustainable City'*

22. No development hereby permitted shall take place until full engineering and construction details (based on approved plan 02427-A-004-P0) of the proposed site access and emergency access off Bennetts Road North have been submitted to and approved in writing by the local planning authority and no part of the development hereby permitted shall be occupied until the approved works have been completed in accordance with the approved plans. The works shall be retained as approved thereafter.

Reason: *In the interests of highway safety in accordance with the aims and objectives of Policies AC1, AC2 and AC3 of the Coventry Local Plan 2016.*

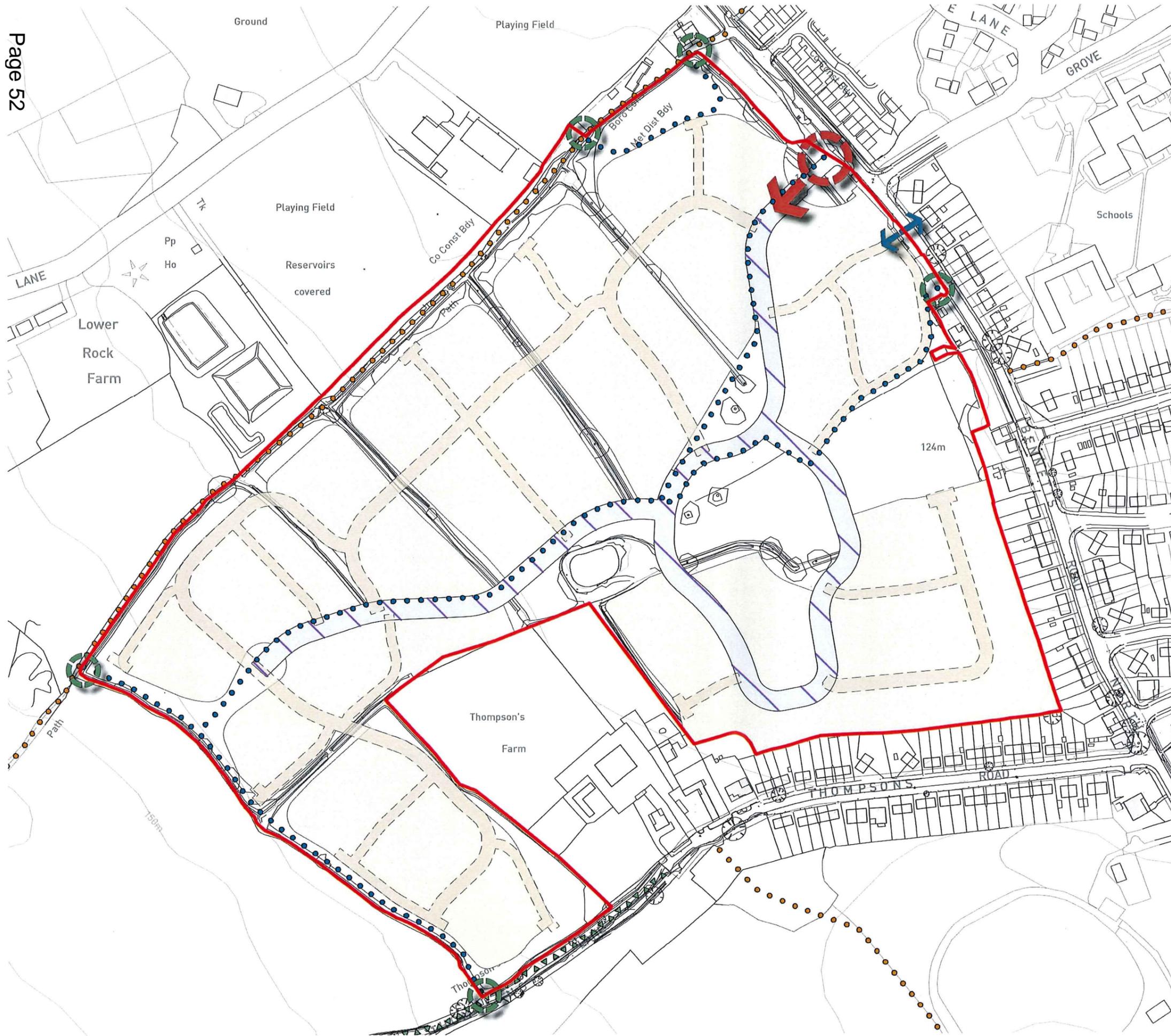
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REFEREMNCE NUMBER
OUT/2019/0484

SITE:
**LAND AT THOMPSONS FARM,
THOMPSONS ROAD**

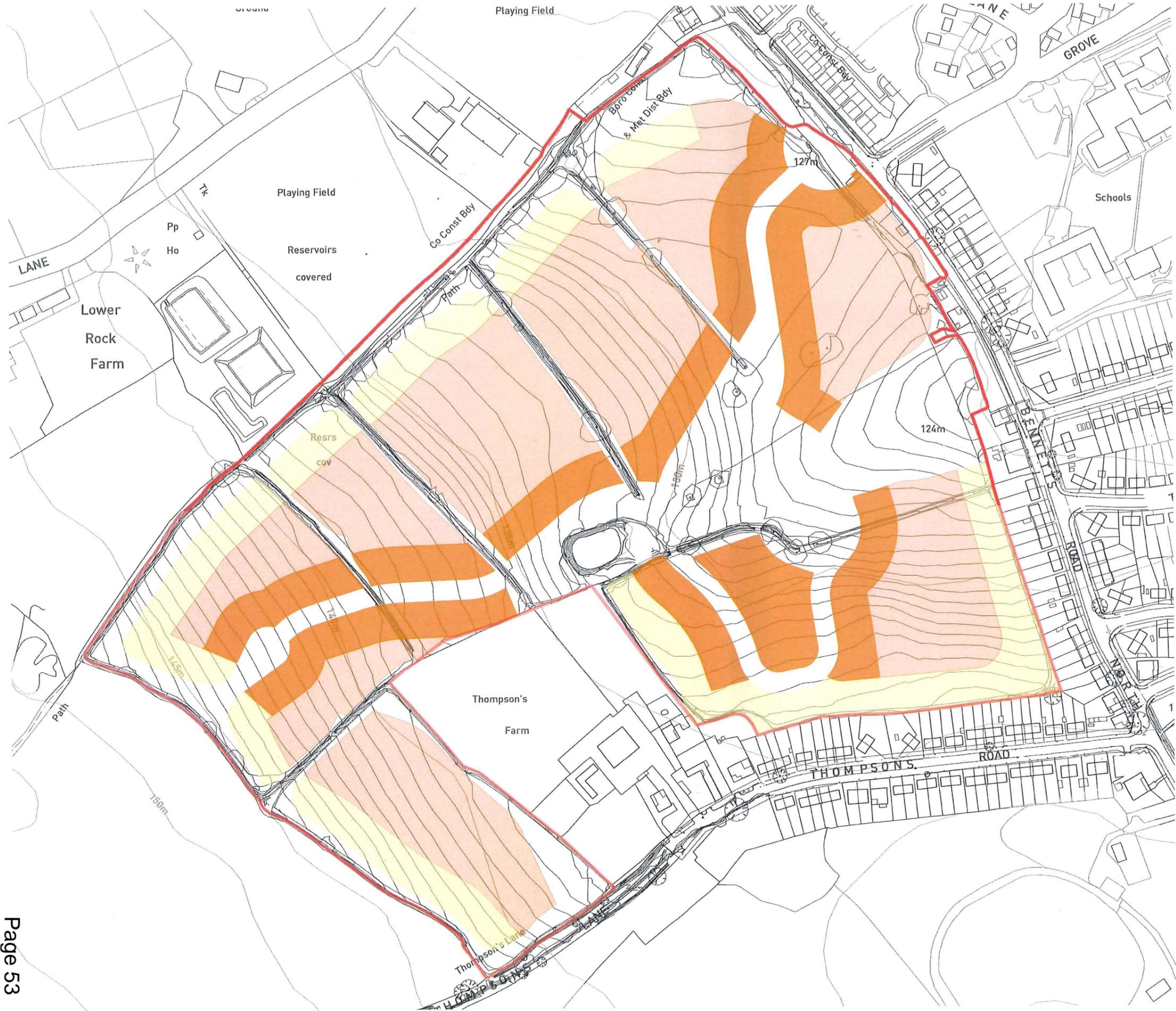
SCALE 1:1250



KEY	
	SITE LOCATION 20.7HA
	DEVELOPMENT PARCELS
	MAIN VEHICULAR ACCESS POINT
	EMERGENCY VEHICULAR ACCESS POINT FROM EXISTING HIGHWAY
	POTENTIAL PEDESTRIAN ACCESS POINTS
	CENTRAL AVENUE CORRIDOR SUBJECT TO DETAILED DESIGN
	INDICATIVE SECONDARY & TERTIARY ROADS SUBJECT TO DETAILED DESIGN
	EXISTING PUBLIC RIGHTS OF WAY (PROW)
	PENDING PROW APPLICATION (REF:CAP 264)
	POTENTIAL NEW PEDESTRIAN LINKS

4.2 FIGURE
 Access & Movement
 Parameter Plan TITLE

1:2500 @ A3 SCALE
 P16-0926_10-01D DWG. NO



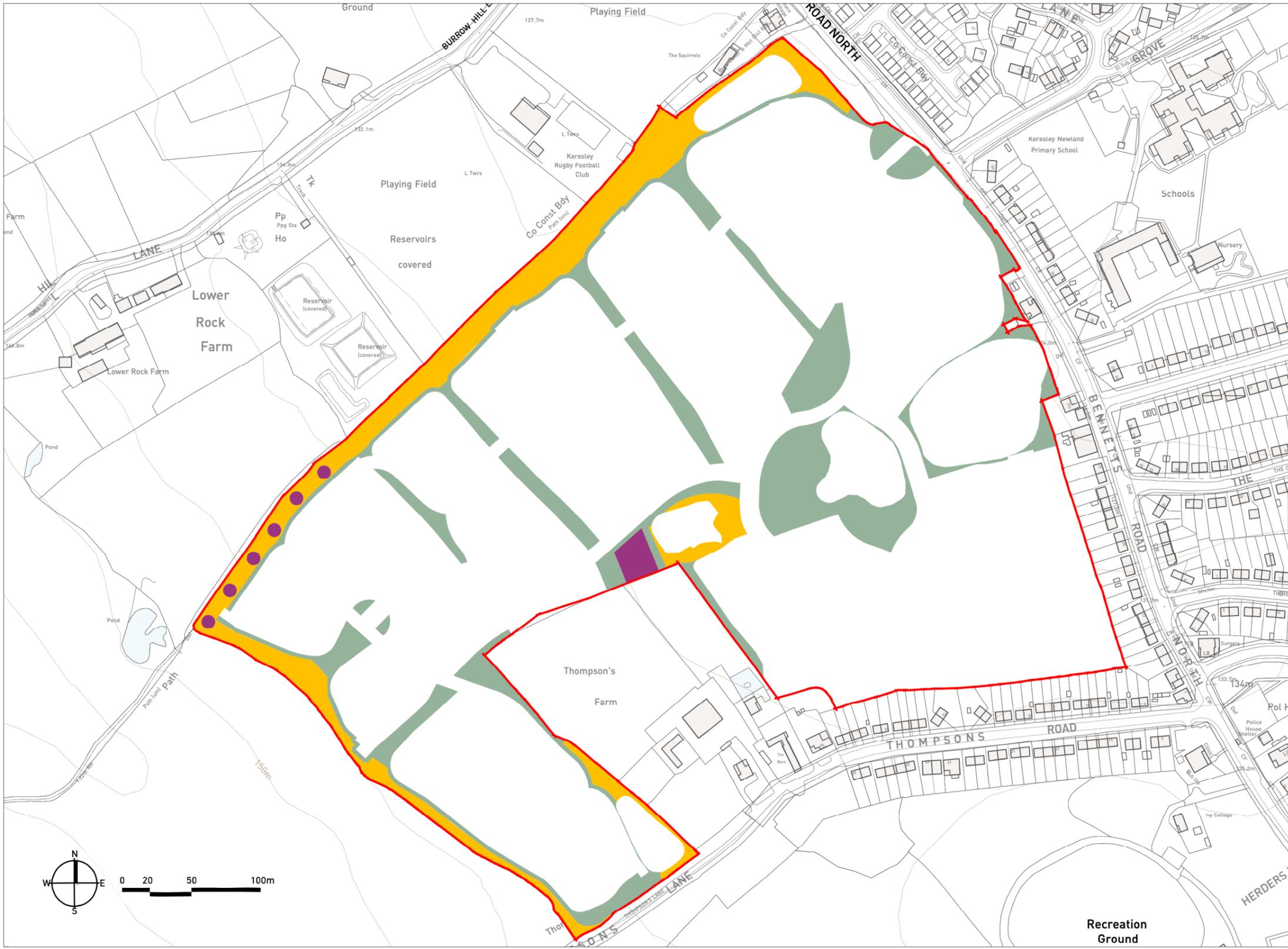
KEY

- SITE LOCATION 20.7HA
- PREDOMINANTLY 2 STOREY BUILDINGS. MAXIMUM BUILDING HEIGHT UP TO 10M ABOVE THE FUTURE GROUND LEVEL HEIGHT*
- PREDOMINANTLY 2 STOREY WITH POTENTIAL FOR UP TO 2.5 STOREY AROUND KEY BUILDINGS & NODAL POINTS. MAXIMUM BUILDING HEIGHT UP TO 10M ABOVE THE FUTURE GROUND LEVEL HEIGHT*
- PREDOMINANTLY 2 STOREY WITH POTENTIAL FOR UP TO 3 STOREY AROUND KEY BUILDINGS & NODAL POINTS. MAXIMUM BUILDING HEIGHT UP TO 13M ABOVE THE FUTURE GROUND LEVEL HEIGHT*
- EXISTING CONTOURS (AT 0.5M INTERVALS)

* FUTURE GROUND LEVEL ALLOWS FOR A MAXIMUM OF 1.5M ABOVE OR BELOW THE EXISTING GROUND LEVEL

4.3 FIGURE
Building Heights TITLE
Parameter Plan

1:2500 @ A3 SCALE
 P16-0926_10-02B DWG. NO



KEY

- SITE BOUNDARY
- PARKS AND OPEN SPACE (2.38HA)
- ACCESSIBLE NATURAL GREEN SPACE (1.51HA)
- PLAY SPACE (0.12HA)

LAND NORTH OF THOMPSONS FARM, KERESLEY - PUBLIC OPEN SPACE PLAN





KEY

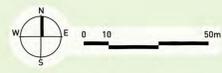
-  SITE LOCATION 20.7HA
-  PROPOSED RESIDENTIAL AREAS (15.3HA)

LANDSCAPING/GREEN INFRASTRUCTURE

-  PROPOSED GREEN SPACE
-  POTENTIAL PLAY AREA
-  POTENTIAL DRAINAGE FEATURES
-  RETAINED VEGETATION (SUBJECT TO DETAILED PROPOSALS)
-  INDICATIVE NEW PLANTING (SUBJECT TO DETAILED PROPOSALS)
-  EXISTING POND

ACCESS & MOVEMENT

-  PROPOSED MAIN ACCESS
-  PROPOSED EMERGENCY ACCESS
-  PROPOSED MAIN AVENUES
-  PROPOSED INTERNAL ROADS
-  PROPOSED EDGE ROADS
-  PROPOSED LANES
-  PROPOSED PRIVATE DRIVES
-  KEY NODAL SPACES
-  EXISTING PUBLIC RIGHTS OF WAY (PROW)
-  POTENTIAL FOOTPATHS
-  PENDING PROW APPLICATION (REF. CAP 264)
-  POTENTIAL LOCATION FOR BUS STOP/LAYOVER (SUBJECT TO DETAILED PROPOSALS)



LAND NORTH OF THOMPSONS FARM, KERESLEY - INDICATIVE CONCEPT MASTERPLAN





- KEY**
- SITE LOCATION 20.7HA
 - PROPOSED DEVELOPMENT PARCELS TO INCLUDE UP TO 500 DWELLINGS, INCIDENTAL OPEN SPACE, ROADS & PARKING SUBJECT TO DETAIL
 - CENTRAL AVENUE CORRIDOR SUBJECT TO DETAILED DESIGN
 - GREEN INFRASTRUCTURE (GI)
 - ★ INDICATIVE LOCATION OF PLAY AREA
 - ★ INDICATIVE LOCATIONS OF ATTENUATION BASINS SUBJECT TO DETAILED DRAINAGE DESIGN
 - EXISTING VEGETATION TO BE RETAINED WHERE POSSIBLE (WITHIN GI)
 - EXISTING POND TO BE RETAINED WHERE POSSIBLE

CITY DEVELOPMENT
DIRECTORATE
16 OCT 2019
DEVELOPMENT
MANAGEMENT

<p>4.1 Land Use Parameter Plan</p>	<p>FIGURE TITLE</p>
<p>1:2500 @ A3 P16-0926_10-03D</p>	<p>SCALE DWG. NO</p>

Planning Committee Report	
Planning Ref:	FUL/2020/2794
Site:	Lyons Park, Former Lawrence Automotive Site, Sayer Drive, CV5 9PF
Ward:	Bablake
Proposal:	Construction of an industrial unit (General Industrial (Class B2) and/or Storage and Distribution (Class B8) use) with ancillary offices, access, servicing, parking, drainage and landscaping. Construction of HGV trailer parking for adjacent unit (Class B8) with pre-check offices, lighting, drainage and landscaping; and all other ancillary works.
Case Officer:	Ayesha Saleem

SUMMARY

The application proposes the erection of an industrial unit (General Industrial (Class B2) and/or Storage and Distribution (Class B8) use) with ancillary offices, access, servicing, parking, drainage and landscaping and construction of HGV trailer parking for adjacent unit (Class B8). The proposal is considered to have an acceptable impact upon the street scene, no detrimental impact upon the existing neighbouring properties and no detrimental impact upon the Green Belt.

BACKGROUND

The application has been recommended for approval. The application has received more than 5 public representations objecting to the proposal

KEY FACTS

Reason for report to committee:	More than 5 objections have been received
Current use of site:	The site had existing industrial buildings with associated parking and hardstanding on site.
Proposed use of site:	Erection of an industrial B2 and B8 use with associated offices and construction of HGV trailer parking for the existing adjacent unit.

RECOMMENDATION

Planning committee are recommended to approve planning permission subject to conditions and the signing of a s106 agreement to secure Biodiversity Offsetting and a Travel Plan Monitoring contribution.

REASON FOR DECISION

- The proposal is acceptable in principle.
- The proposal will not adversely impact upon highway safety.
- The proposal will not adversely impact upon the amenity of neighbours.

- The proposal accords with Policies DS3, JE2, JE5, GB1, GE3, GE4, DE1, AC1, AC2, AC3, AC4, EM4, EM5, EM6, EM7 and IM1 of the Coventry Local Plan 2016, together with the aims of the NPPF.

BACKGROUND

APPLICATION PROPOSAL

The application comprises of the erection of a new commercial unit (General industrial (Use Class B2) and/or Storage and Distribution (Class B8)) with ancillary offices, access, servicing, parking, drainage, and landscaping. It also seeks planning permission for the formation of an area to accommodate additional HGV trailer parking spaces for use in connection with the adjacent existing Amazon operation.

The application relates to a phased development, with each of the two elements described above forming a separate phase. The first phase relates to the HGV parking for the adjacent amazon unit and the second phase relates to the new industrial unit.

The HGV trailer parking area (comprising approximately 2.28 hectares of the Site), would provide 185 trailer parking spaces alongside two pre-check offices with barriers and associated lighting. The HGV trailer parking will be accessed from Coundon Wedge Drive to serve HGV movements. In terms of how it will operate, inbound HGV's would check in at the gatehouse and drop the inbound trailer in the proposed yard, which generally contains only a limited number of products. The HGV will then collect a full trailer, likely full of multiple products and depart with minimal delay. The inbound trailer will then be towed around the Site using a small 'Tug'. The trailer will then be unloaded into the warehouse and re-filled. Finally, the trailer will be moved to the trailer park before being collected by an HGV (which has just dropped off an inbound trailer) and dispatched. The proposed gatehouse is to be located south within the site at the entrance for the HVG parking. The building is to measure 3.6 mete by 8.6 metres and a height of circa 2.9 metres.

The proposed industrial unit comprises 9,861m² (GIA) of floorspace. This consists of 8,604m² of warehouse and 1,257m² of ancillary office/welfare floorspace. There is also 13m² of floorspace within the two pre-check offices. The maximum ridge height of the Unit will be 15.45 metres, with 12.5 metres to haunch. The industrial unit will be designed in cladding using a palette of grey hues and the office element would be designed in a different colouring cladding with elements of glazing. This site would have adequate parking/cycle parking provision as well as level access and yard space with manoeuvrability for large HGVs. There will be 2 level access doors and 10 dock levellers. The proposed development will be operated on a 24-hour basis

The parking for the industrial unit would be located to the east of the building, separate from the yard area for safety reasons. The ancillary office area would be located on the north eastern corner to allow natural surveillance and would be accessed from the car park. The yard, and dock levellers, would be located to the north of the building, facing into the existing industrial estate, with access taken off the existing estate road to the north

The open area to the south west of the Site is proposed to accommodate an attenuation pond which will have side slopes facing the pond set at a 1 in 3 gradients for safe

maintenance. Slopes facing the River Sherbourne have been designed at a 1 in 5 gradient, to match the existing topography and provide a higher safety factor with regards to slope stability. Finally, a minimum of 300mm freeboard is provided from the critical, 100 year plus 40% climate change storm event water level to the top of the pond bank. Water will be conveyed to the pond where it will be attenuated and discharged at the pre-development greenfield run-off rates, up to the 100-year storm event. Foul water will be conveyed to the existing 300mm foul sewer within Lyons Drive, to the west of the Site. It is noted that the pond sits within the Green Belt and the Butts Lane Meadows Local Wildlife Site.

Planting would be included along the frontage of the Site, facing Lyons Park Drive and surrounding the parking areas to have a softening impact. There would also be a small area at the entrance to the HGV Parking Area. A more substantial area of planting/landscaping is also proposed within the attenuation area. This landscaping would consist of new tree planting, with thicket, hedgerow, grass, wildflower, wet wildflower and aquatic planting also proposed.

SITE DESCRIPTION

The application site comprises mainly of brownfield land with part falling within the Green Belt. The application site sits immediately adjacent to the existing Lyons Park industrial area, which is an employment allocation. Consequently, the areas to the north, east and west have an industrial character and include large commercial units, such as the Amazon facility that lies immediately adjacent.

The Site is accessed off the existing Lyons Park estate roads, which lead on to Coundon Wedge Drive. An additional access is provided off Sayer Drive, which is a private Road linked to the entrance of the wider industrial site.

The area to the south of the Site is designated as open countryside, Green Belt and forms part of the Butts Lane Meadows and Coundon Wedge Local Wildlife Sites.

There is an area of screen planting to the west of the Site, which is subject to a Tree Preservation Order. There are four trees which are subject to a Tree Preservation Order within the area proposed for attenuation.

PLANNING HISTORY

Application Number:	Description of development:	Decision and date:
R/2009/0669	Outline application for redevelopment of former Jaguar site with a mixed-use proposal including B1, B2 units and residential development, (outline application discharging means of access comprising internal road network and engineering operations for onsite groundworks)	Approved 23/12/2009

R/2010/0861	Variation of condition No. 36 imposed on planning permission reference: R/2009/0669 granted on 23/12/2009 for redevelopment of the former Jaguar site with a mixed-use proposal including: B1, B2 units and residential development - to remove reference to the submission of a Travel Plan in respect of the residential element of the scheme	Approved 13/08/2018
S73/2010/1699	Variation of condition 30 imposed on planning permission reference R/2009/0669 granted on 23/12/2009 for the redevelopment of former Jaguar site with a mixed-use proposal including B1, B2 units and residential development, (outline application discharging means of access comprising internal road network and engineering operations for onsite groundworks), to allow occupation of industrial/commercial units prior to the provision of cycle path	Approved 07/06/2011
RMM/2014/1796	Submission of reserved matters details for redevelopment of part of the former Jaguar Browns Lane site (access from estate road, appearance, landscaping, layout and scale) in respect of the erection of an industrial building (Use Class B2) with ancillary office/storage areas and associated access, parking, landscaping and curtilage structures/plant pursuant to outline planning permission reference 55011 granted on the 23 December 2009 for a mixed-use commercial & residential redevelopment (an Environmental Impact Assessment was not required at outline stage)	Approved 16/09/2014
RMM/2014/2068	Submission of reserved matters details for redevelopment of part of the former Jaguar Browns Lane site (access from estate road, appearance, landscaping, layout and scale) in respect of the erection of 5 no. industrial units (Use Class B2) with ancillary office/storage areas and associated access, parking, landscaping and curtilage structures/plant pursuant to outline planning permission reference 55011 granted on the 23 December 2009 for a mixed-use commercial & residential redevelopment (an Environmental Impact Assessment was not required at outline stage)	Approved 16/09/2014
FM/2014/2105	Development of 5 no. general industrial (Use Class B2) and/or storage and distribution (Use Class B8) units with ancillary office areas and associated	Approved 16/09/2014

	access, service yards, parking, landscaping and curtilage structures/plant	
FUL/2016/0627	Erection of an industrial / warehouse unit (Use Class B2 - General Industry/B8 - Storage and Distribution) with ancillary offices and plant, associated infrastructure including service yard, access, parking, landscaping and associated works	Approved 03/06/2016
FUL/2016/2397	Erection of a storage and distribution unit (Use Class B8) with ancillary offices, mezzanine floor, gatehouse, plant and associated infrastructure including service yard, internal roads, parking, vehicle pick-up/ drop-off, landscaping, boundary treatment, hardstanding and related works	Approved 19/12/2016
S73/2017/0902	Variation of Condition 2 (drawing numbers) and Condition 16 (updated noise report) imposed on planning permission reference FUL/2016/2397 granted planning approval on 19.12.2016 for the erection of a storage and distribution unit (use class B8) with ancillary offices, mezzanine floor, gatehouse, plant and associated infrastructure including service yard, internal roads, parking, vehicle pick-up/drop-off, landscaping, boundary treatment, hardstanding and related works	Approved 03/08/2017
DEMN/2020/0206	Application for Prior Notification of proposed demolition	Details Required 24/02/20
DEMN/2020/0751	Prior Notification of proposed demolition	Details Not Required 17/04/20

POLICY

National Policy Guidance

National Planning Policy Framework (NPPF). The NPPF published in February 2019 sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that is relevant, proportionate and necessary to do so. The NPPF promotes sustainable development and good design is recognised as a key aspect of this.

The National Planning Practice Guidance (NPPG) 2014, this adds further context to the NPPF, and it is intended that the two documents are read together.

Local Policy Guidance

The current local policy is provided within the Coventry Local Plan 2016, which was adopted by Coventry City Council on 6th December 2017. Relevant policies relating to this application are:

Policy DS3: Sustainable Development
Policy DS4 (a): General Masterplan Principles
Policy JE2: Provision of Employment Land and Premises
Policy JE5: Location of R&D, Industrial and Storage/Distribution Development
Policy GB1: Green Belt and Local Green Space
Policy GE3: Biodiversity, Geological, Landscape and Archaeological Conservation
Policy GE4: Tree Protection
Policy DE1: Ensuring High Quality Design
Policy AC1: Accessible Transport Network
Policy AC2: Road Network
Policy AC3: Demand Management
Policy AC4: Walking and Cycling
Policy EM4: Flood Risk Management
Policy EM5: Sustainable Drainage Systems (SuDS)
Policy EM6: Redevelopment of Previously Developed Land
Policy EM7: Air Quality
Policy HW1: Health Impact Assessments (HIA)
Policy IM1: Developer Contributions for Infrastructure

Supplementary Planning Guidance/ Documents (SPG/ SPD):

SPD Delivering a More Sustainable City 2009
SPD Coventry Connected (Transport and Accessibility) 2019
SPD Air Quality 2019
Design Guideline for Developments in Coventry's Ancient Arden – A Historic Landscape Area 1995

STATUTORY CONSULTATION RESPONSES

Highways (CCC)- No objections subject to conditions and informatives
Environmental Protection (CCC)- No objections subject to conditions
Planning Policy (CCC)- No objections
Ecology (CCC)- No objections subject to conditions
Drainage (CCC)- No objections subject to conditions
Trees (CCC)- No objections subject to conditions
Urban Design (CCC)- No objections
Archaeology (CCC)- No objections subject to condition
Economic Development Services (CCC)- No objections subject to condition
Environment Agency- No objections subject to condition
West Midlands Fire Services- No objections subject to informative

PUBLIC RESPONSES

Notification letters were sent out to adjoining neighbouring houses and two site notices were displayed on the 27th November 2020.

6 letters of objections and 2 not object representations were received raising the following material planning considerations:

- a) Roads not built for HVG's
- b) Increased traffic

- c) Increased noise pollution
- d) Increased litter
- e) Local wildlife to be impacted
- f) Detrimental impact upon air quality
- g) No notification/ letter sent to a resident
- h) Attenuation pond close to a residential property may impact upon a neighbour's land
- i) Fence should be in place for noise barrier
- j) More signage is needed to stop trailers to turn on residential properties
- k) Long queues of HVG's in front of other units
- l) Conditions should be added in regard to site signage strategy, trailer parking management and HVG routing strategy

Any further comments received will be reported within late representations.

ASSESSMENT

Principle of Development:

The site is an existing employment use that sits adjacent to an allocation which extends the employment use. This proposal would retain the use in employment and is acceptable in principle.

Whilst it is not within the allocated area set out under Policy JE2 (Provision of Employment Land and Premises), the Policy requires a minimum supply of new employment land (58hectares) on a 5-year rolling cycle that will be achieved through a combination of newly allocated sites and recycled land. The proposal, in our view represents the recycling of land, given it is a brownfield site that contains functionally obsolete buildings, particularly given the planning consent for their demolition. Therefore, officers consider the proposal a sensible location for contributing to this rolling supply. Indeed, the National Planning Policy Framework (NPPF) supports the re-use of previously developed land in Paragraph 117 and, in Paragraph 118, goes on to state that 'substantial weight' should be given to the value of using suitable underutilised brownfield land to meet identified development needs.

In addition to the above, officers consider that the proposed development would be acceptable in principle given the provisions of Policy JE5 (Location of R&D, Industrial and storage/Distribution Development), which allows for industrial and storage/distribution development (including changes of use and the expansion of existing operations) on non-allocated sites subject to meeting certain criteria. Planning policy officers are satisfied that the proposal meets the policy requirements and the proposal would complement other allocated employment areas nearby.

The area of the proposal that falls within the Green Belt does not include any buildings, but It will provide drainage attenuation, which is classed as an engineering operation. Paragraph 146 confirms that engineering operations are not 'inappropriate development'

within the Green Belt provided they preserve its openness and do not conflict with the purposes of including land within it.

To conclude, the principle of the proposed development is in line with Coventry Local Plan Policies JE2 and JE5 and is supported in principle at this time.

Impact on visual amenity:

Policy DE1 of the Local Plan seeks to ensure high quality design and development proposals must respect and enhance their surroundings and positively contribute towards the local identity and character of an area.

Chapter 12 of the NPPF requires that planning should always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings.

When considering this proposal, it was important to consider the Coventry City Council Local Plan with reference to Policy DE1 and the National Design Guide 2019. The proposal suggests the development of 3 Commercial units on an existing Brownfield site.

Due to the intended layout of the proposals the massing is a positive aspect with the predominant massing being situated in the centre of Lyons park and masked by existing similar scaled units adjacent to the site to reduce potential wider visual intrusion of the development in the locality. The layout has positively considered inclusive access design principles with the proposal through the inclusion of level access, disabled parking allocation, disabled facilities and Lift provision, which is further assisted by the open plan format of the office units.

Furthermore, the landscaping is also a beneficial aspect of this proposal with the inclusion of soft landscaping boundary treatments and notably at key access points to delivery an appropriate status to the sense of arrival that also assists in providing improved site legibility for users. There has also been beneficial consideration of boundary treatment which has been demonstrated within the proposal through the provision of a 2.4-meter mesh fencing. Further additional positive consideration has also been given to providing secure cycle storage for the intended users which would be supported by the Coventry Connected SPD.

Further beneficial consideration has also been given to the provision of secure bin storage allocation on site, which has also been masked from the Streetscene and would therefore be supported by the Waste Storage Technical Advice Note.

The use of the existing green buffer to produce capacity for a SUDS is a positive element of considering sustainability at the earliest stage which would be supported by Policy DE1 and the National Design Guide 2019.

The Design and materiality are also positive aspects of this scheme with a pallet which is reflective of the existing adjacent units with Lyons Park and would form a continuation of pallet on the street and would be supported by Policy DE1. Urban Design officers have no objection to the principle of development for this site.

Impact on residential amenity:

The closest residential property to the application site is Highfield House 101 Staircase Lane. The property is located circa 90.1 metres away from the access road to the south of the site. The neighbouring property is surrounded by trees. Given the significant distance it is considered that the proposed development will not have a detrimental impact upon the amenity of this occupant. To ensure there is no detrimental impact in relation to noise, figure 6.1 in the noise assessment identifies the potential acoustic barrier on the south east boundary. The external finishes plan shows a fence on the south east boundary.

Environmental Protection officers have also recommended a condition in regard to a noise management plan to ensure that there will be no detrimental impact upon the amenity of residential property close by given the additional HGV parking.

Highway considerations:

The Highway Authority have reviewed the information supplied alongside the application and consider that the impacts of the development are not severe and has no objections to the current proposal subject to conditions in relation to details of cycle parking, car parking and turning areas to be laid out, construction management plan and details in relation to footway crossing and informatives.

Highway officers recognise there may be concerns in relation to queuing of HGV vehicles therefore appropriate conditions have been recommended to ensure appropriate road signage, HGV routing and trailer parking management. A Travel plan has been submitted with the application and this is to be conditioned.

Other Matters:

Environmental Protection officers have no objections to the proposals subject to conditions in relation to a noise management plan, (as mentioned above) contaminated land and air quality in relation to vehicle charging points and a site-specific construction environmental management plan.

The Environment Agency have no objections to the proposals subject to a condition in relation to the protection of controlled waters.

The Councils Drainage officers have no objections to the proposals subject to conditions in which officers have recommended to ensure a satisfactory drainage solution.

The Council's Tree officer has assessed the reports that have been submitted and is satisfied with the information submitted subject to conditions in relation to safeguarding TPO trees, tree protection details and site monitoring (to be included in the Arboricultural Method Statement) to be conditioned.

The site includes part of Bull Lane Meadows Local Wildlife Site and is immediately adjacent to Coundon Wedge Grasslands Local Wildlife Site. A number of further LWS fall within a 1km radius but would not be directly affected. The Councils Ecology officer has assessed the Reports that have been submitted. The area of retained grassland has been increased and areas of proposed scrub planting reduced, and this is welcomed. The priority is to safeguard and enhance the grassland Local Wildlife Site. An area of grassland will be lost to create a SuDS feature. Ecology does not accept the details of the submitted BIA. However, the BIA is a guideline and ecology does accept that the loss can be mitigated by improved grassland management and additional habitat in other parts of the wider site. A habitat management plan will be submitted which will provide details of long-term enhancement of the grassland. There is a preference to landscape the SuDS feature with planting. Ponds are a habitat which provides immediate biodiversity benefit, allowing natural colonisation and permits local biodiversity to establish. However, there is no objection to landscaping the pond, subject to a detailed planting scheme being provided. Overall, Ecology officers have confirmed that on-site biodiversity offsetting is to be provided and part of the S106 agreement.

Within the site itself, there is a general dearth of known activity aside from the North Field House site, which was not demolished until fairly recently. Historically, the land appears to have been agricultural lands, with no substantial settlement recorded nearby. There is a low potential for anything other than remains of an agricultural origin within the site and consequently the significance of any remains is not likely to be more than local importance. The presence of relict topsoil, surviving at varying depths below the present ground surface, may be of some interest, depending on the date of these deposits. It is possible that datable material has survived within these deposits and thus it is deemed worthwhile that archaeological monitoring of groundworks is undertaken as a condition of planning. The Archaeology officer has no objections to the proposals subject to a condition in relation to a written scheme of investigation.

Economic Development Services have no objections to the proposals subject to a condition in relation to local employment.

In relation to the neighbour's comments regarding the attenuation pond being close to a residential property which may impact upon a neighbour's land. Drainage officers have confirmed that the proposed attenuation pond is located such that the current abstraction operations will not be affected by the flow of surface water which will be towards the River Sherbourne and away from the existing residential development.

The neighbour representation received regarding not being consulted on the application was sent a neighbour letter and also two site notices were placed outside the application site.

S106 Contributions:

The proposed development involves the creation of a SuDS feature within the Local Wildlife Site and associated landscaping. The Landscape Concept Plan (Barry Chin, October 2020, 2 sheets) has been revised following discussions with the City Council. The revisions included reducing tree and shrub planting within the LWS in order to retain the grassland habitats and additional habitat creation in other parts of the site including wildflower grassland. The SuDS is designed as a biodiversity-rich feature which, although different from the grassland, will have significant value and provide opportunities for wildlife. The revised BIA including the landscaping within Bull Lane Meadow LWS and other parts of the site calculates an overall gain of 2.14 units. The mitigation requires the enhanced, sympathetic management of remaining grassland areas, the creation of a wildlife friendly SuDS with associated habitats and small areas of new habitat in other parts of the site. Overall, there is a small loss of woodland habitat and a larger gain in wetland habitat. The habitats will be managed for a 30-year period which will allow them to become fully established. The information provided establishes that the development will lead to an overall gain in biodiversity. The proposed mitigation fully offsets the loss of 5.77 units and there is no requirement for any contribution for offsetting projects at other sites. Ecology officers have confirmed that on-site biodiversity offsetting is to be provided and part of the S106 agreement.

Highways have requested a S106 contribution for the Travel Plan Monitoring Contribution of £5000.

Equality implications:

Section 149 of the Equality Act 2010 created the public sector equality duty. Section 149 states: -

(1) A public authority must, in the exercise of its functions, have due regard to the need to:

- a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
- b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

Officers have taken this into account and given due regard to this statutory duty, and the matters specified in Section 149 of the Equality Act 2010 in the determination of this application.

There are no known equality implications arising directly from this development.

CONCLUSION

In view of the acceptable principle, design and no detrimental impact upon neighbouring properties, the scheme accords with development plan policies, supplementary planning guidelines and the NPPF 2019 therefore the application is recommended for approval. The development is in accordance with Policies DS3, JE2, JE5, GB1, GE3, GE4, DE1, AC1, AC2, AC3, AC4, EM4, EM5, EM6, EM7 and IM1 of the Coventry Local Plan 2016.

CONDITIONS:/REASON

1. The development hereby permitted shall begin not later than 3 years from the date of this decision.

Reason: *To conform with Section 91 of the Town and Country Planning Act 1990 (as amended)*

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Dwg:3726-1-100-P6-Site Plan
Dwg:3726-1-101-P3 Existing Site Plan
Dwg:3726-1-102-P3 Location Plan
Dwg:3726-1-103-P7-External Finishes Plan
Dwg:3726-1-202-P6 Proposed Roof Plan
Dwg:3726-1-300-P6 Proposed Unit Elevations
Dwg:3726-1-203-P3-Pre-Check Gatehouse Plans & Elevations
Dwg:3726-1-201-P4 Office Plans
Dwg:3726-1-200-P5 Unit Plan
Dwg:3726-1-106-P2 Cycle Shelter Plan
Dwg: LP-LE-GEN-XX-SK-CE-027-S5-A1-A Road Improvements
Dwg: CPW-200778-E-200-EXT-0001-P3 Proposed External Lighting Layout
Dwg:2045-20-01B Tree constraints plan 1 of 2 29-10-20
Dwg:2045-20-02B Tree constraints plans 2 of 2 29-10-20
Dwg:2045-20-03D - Tree Protection, Retention + Removal Plan
Dwg:2045-20-04D - Tree Protection, Retention + Removal Plan
Dwg: 2045-20-05H - Landscape Concept Plan (Sheet 1 of 2)
Dwg:2045-20-06F - Landscape Concept Plan (Sheet 2 of 2)
Dwg:3726-1-105-P5-Phasing Plan
Travel Plan- October 2020
Statement of Community Involvement- October 2020
Environmental Site Assessment- November 2020
Design and Access Statement-November 2020
Energy Assessment- November 2020 06/11/2020 Issue No. 1
Air Quality Assessment- November 2020
Noise Assessment- November 2020

Drainage Strategy- October 2020
Planning Statement- November 2020
Transport Statement- November 2020
Preliminary Ecological Appraisal- November 2020
Arboricultural Impact Assessment- 465-20 (AIA), Revision 0
Arboricultural Assessment Addendum- January 2021
Archaeological DBA- November 2020
Geo-Environmental Assessment- August 2019
Biodiversity Impact Assessment Calc (Final)
External Lighting Report- Rev P2
Lyons Park Consultation Response Letter- 1st February 2021
Highway Comments Response- 12th March 2021

Reason: *For the avoidance of doubt and in the interests of proper planning*

3. Prior to their incorporation into the development hereby permitted, sample details of all facing and roofing materials shall be submitted to and approved in writing by the Local Planning Authority. These details shall be installed only in full accordance with the approved details prior to the first occupation of the development within that phase and thereafter shall be retained and shall not be removed or altered in any way.

Reason: *To ensure that the proposed development has a satisfactory external appearance in the interests of the visual amenities of the area in accordance with Policy DE1 of the Coventry Local Plan 2016.*

4. Any landscaping (other than the planting of trees and shrubs) including the erection of boundary treatment, and the installation of paving and footpaths shown on the approved plans shall be completed in all respects within three months of the first use of the development within that phase and the tree(s) and shrub(s) shall be planted within the first planting season following that first use. Any tree(s) or shrub(s) removed, dying, or becoming; in the opinion of the Local Planning Authority; seriously damaged, defective or diseased within five years from the substantial completion of the scheme shall be replaced within the next planting season by tree(s) or shrub(s) of similar size and species to those originally required to be planted. All hedging, tree(s) and shrub(s) shall be planted in accordance with British Standard BS 8545:2014 Trees: from nursery to independence in the landscape - Recommendations and BS4428 - Code of Practice for General Landscape Operations.

Reason: *To ensure a satisfactory standard of appearance of the development in the interests of the visual amenities of the area in accordance with Policies GE1 and DE1 of the Coventry Local Plan 2016.*

5. The development hereby permitted shall not be occupied unless and until the bin storage areas have been provided in full accordance with the details shown on the

approved plans within that phase and thereafter they shall remain available for use at all times and shall not be removed or altered in any way.

Reason: *In the interests of the amenities of the future occupants of the residential accommodation in accordance with Policy DE1 of the Coventry Local Plan 2016.*

6. No storage shall take place in the open on any part of the site.

Reason: *To ensure a satisfactory standard of appearance of the development in the interests of the visual amenities of the area in accordance with Policy DE1 of the Coventry Local Plan 2016.*

7. No machinery shall be operated outside of the building(s)

Reason: *To safeguard the amenities of nearby occupiers in accordance with Policy DE1 of the Coventry Local Plan 2016*

8. The development hereby permitted shall only proceed in strict accordance with a scheme for targeting and utilising local people for construction and employment, which shall be submitted to an approved in writing by the Local Planning Authority.

Reason: *To secure local employment in accordance with the City Council jobs strategy and Policy JE7 of the Coventry Local Plan 2016.*

9. No development (including any demolition or preparatory works) within that phase shall take place unless and until a written scheme of archaeological investigation has been submitted to and approved in writing by local planning authority, which shall include the following:

The programme and methodology of site investigation and recording and the nomination of a competent person(s) or organisation to undertake the agreed works
The programme for post-investigation assessment and subsequent analysis, publication & dissemination and deposition of resulting material. This part of the condition shall not be discharged until these elements have been fulfilled in accordance with the programme set out in the WSI'

The development shall only proceed in full accordance with these approved details.

Reason: *The submission of these details prior to the commencement of development is fundamental to mitigate the effect of the works associated with the development upon any heritage assets and to ensure that information regarding these heritage assets is preserved by record for this and future generations in accordance with Policy HE2 of the Coventry Local Plan 2016*

10. Prior to the occupation of the development within that phase hereby permitted the following drainage details shall be submitted to and approved in writing by the Local Planning Authority:

I. Provisions must be made for the drainage of the site to ensure there are no

temporary increases in flood risk, on or off site, during the construction phase.

II. An appropriately scaled intrusive ground investigation report to establish the depth and type of strata, including percolation results in accordance with BRE 365 and the presence and risk associated with migrant contaminants. Provide evidence of existing groundwater levels and seasonal variation, in order to inform the drainage design

III. The development must be considered for the implementation of permeable paving or similar permeable material for the partial reduction of flow and the improvement of water quality in accordance with Coventry City Council's adopted Supplementary Planning Document for 'Delivering a More Sustainable City'.

IV. The development shall not be occupied until such time as a scheme to install vehicular traffic pollution control measures within the trafficked external areas and at the loading dock leveller, together with oil and petrol separators with high level alarm and a suitable shut-off valve for the wider site, has been submitted to, and approved by, the Local Planning Authority. This should be submitted along with a periodic maintenance plan.

V. Consideration should be given to bring the top water level of the attenuation pond below the existing ground levels, to provide adequate protection against failure. If this is not feasible, a satisfactory geotechnical design should be submitted, supported with calculations demonstrating compliance with the relevant Eurocode Factors of Safety for water retaining structures, with the relevant design certificates to demonstrate this. Additionally, the embankment must allow for safe overtopping and safe routing of surface water to the public highway or public open space, to protect downstream homes and commercial premises.

VI. Provisions must be made for the drainage of the site to ensure there is no discharge of surface water to the Public Highway.

VII. Foul drainage plans

The drainage details shall be installed in full accordance with the approved documentation prior to occupation of the development and thereafter shall be maintained and shall not be removed or altered in any way.

Reason: *To ensure that adequate drainage facilities are available for the satisfactory and proper development of the site in accordance with policies EM1, EM4 and EM5 of the Coventry Local Plan 2016 and Coventry City Council's adopted Supplementary Planning Document for 'Delivering a More Sustainable City'*

11. Prior to their incorporation into the development hereby permitted, a package of measures to minimise the impact of the development upon local air quality shall be submitted to and in writing for each phase by the Local Planning Authority. These measures shall have consideration of the following: -
- (i) Provision for electric vehicle recharging points to be made 'EV-ready' so a power connection is available to install an EV charge point as required;
 - (ii) 1 per 10 unallocated spaces to be for EV re-charging on non-residential development
 - (iii) Use of low NOx boilers (to have a maximum dry NOx emissions rate of 40mg/kWh)

The measures shall be installed in full accordance with the approved details prior to first occupation of the development and thereafter shall be retained and shall not be removed or altered in any way.

Reason: *To mitigate the impacts of development on air quality during and post construction in accordance with Policies DS3 & EM7 of the Coventry Local Plan 2016.*

12. Before the development hereby permitted commences within each phase details of additional gas monitoring shall be submitted to and approved in writing by the local planning authority. If the local planning authority determines the gas protection measures are necessary, the protection measures shall be installed in full accordance with the approved details prior to first occupation of the development and thereafter shall be retained and shall not be removed or altered in any way.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

13. Prior to the commencement of any phase of the development, a method statement detailing the control of emissions into the air during the demolition/construction phase should be submitted to and approved in writing by the Local Planning Authority. The method statement should accord with the Best Practice Guidance - 'The control of dust and emissions from construction and demolition' and include:-
 - a) proposed hours of work;
 - b) map with nearest receptors and distances for dust and noise;
 - c) noise impact on nearest neighbours and control measures as required;
 - d) monitoring methods and measurement locations for dust and noise recording details;
 - e) dust mitigation measures;
 - f) contact details for responsible persons and site personnel training; and
 - g) information provision and liaison with local residents. The development shall only proceed in full accordance with the approved details.

Reason: *To protect the amenity of the occupiers of the residential accommodation hereby approved in accordance with Policies DS3 and EM7 of the Coventry Local Plan 2016.*

14. Prior to the occupation of any phase of the development hereby permitted a noise management plan shall be submitted to and approved in writing by the Local Planning Authority. This shall specify the provisions to be made for the control of noise emanating from the site. The noise mitigation scheme should be installed in full accordance with the approved details prior to the first occupation of that phase and thereafter shall be retained.

Reason: *To safeguard the amenities of the adjoining occupiers of the development in accordance with Policy DE1 of the Coventry Local Plan 2016.*

15. No development or other operations within each phase (including any demolition, site clearance or other preparatory works) shall commence unless and until tree protection measures have been submitted to and approved in writing by the Local Planning Authority.

The tree protection measure shall safeguard against damage or injury during construction works (in accordance with Clause 7 of British Standard BS5837) all trees on the site, or those trees whose root structure may extend within the site and which are the subject of the City of Coventry (Allesley Northfield No.1) Tree Preservation Order 2009 (T1 Monkey Puzzle, T2 Sweet Chestnut, T3 Lime, T4 Lime, T5 Lime, G1 (x2 Oak), G2 (1 Sycamore and 1 Oak) and Woodland) In particular no excavations, site works, stock piling, trenches or channels shall be cut or pipes or services laid, no fires shall be lit within 10 metres of the nearest point of the Root Protection Area or outer canopy of any protected tree(s); no equipment, machinery or structure shall be located within this zone; no mixing of cement or use of other contaminating materials or substances shall take place within, or close enough to, a root protection area that seepage or displacement could cause them to enter a root protection area, or any other works be carried out in such a way as to cause damage or injury to the tree(s) by interference with their root structure and no soil or waste shall be deposited on the land in such a position as to be likely to cause damage or injury to the tree(s).

The approved mitigation and / or protection measures shall be put in place prior to the commencement of any works and shall remain in place during all construction work.

Reason: *To protect adjacent trees which are the subject of a Tree Preservation Order and trees which are of significant amenity value to the area and which would provide an enhanced standard of appearance to the development in accordance with Policy GE1 and GE4 of the Coventry Local Plan 2016.*

16. No development or other operations within each phase (including any demolition, site clearance or other preparatory works) shall commence unless and until the tree protection measures identified in the approved application documentation have been put into place in strict accordance with the approved details and thereafter they shall remain in place during all construction work. In addition no excavations, site works, stock piling, trenches or channels shall be cut or pipes or services laid, no fires shall be lit within 10 metres of the nearest point of the canopy or root protection area of any protected tree(s); no equipment, machinery or structure shall be located within this zone; no mixing of cement or use of other contaminating materials or substances shall take place within, or close enough to, a root protection area that seepage or displacement could cause them to enter a root protection area or any other works carried out in such a way as to cause damage or injury to the tree(s) by interference with their root structure and that no soil or waste shall be deposited on the land in such a position as to be likely to cause damage or injury to the tree(s).

Reason: *To protect those trees which are of significant amenity value to the*

Conservation Area and which would provide an enhanced standard of appearance to the development in accordance with Policy GE3, GE4 and HE2 of the Coventry Local Plan 2016.

17. Notwithstanding the submitted arboricultural impact Assessment, prior to commencement of development or other operations within each phase (including any demolition, site clearance or other preparatory works) a revised arboricultural method statement to include a site monitoring model shall be submitted to and approved in writing by the Local Planning Authority. The approved mitigation and / or protection measures shall be put in place prior to the commencement of any works and shall remain in place during all construction work.

Reason: *To protect those trees which are of significant amenity value to the area and which would provide an enhanced standard of appearance to the development in accordance with Policy GE3 and GE4 of the Coventry Local Plan 2016.*

18. Prior to the commencement of development within any phase approved by this planning permission (or such other date or stage in development as may be agreed in writing with the Local Planning Authority), the following components of a scheme to deal with the risks associated with contamination of the site shall each be submitted to and approved, in writing, by the local planning authority:
 1. A preliminary risk assessment which has identified:
 - o all previous uses
 - o potential contaminants associated with those uses
 - o a conceptual model of the site indicating sources, pathways and receptors
 - o potentially unacceptable risks arising from contamination at the site.
 2. A site investigation scheme, based on (1) to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.
 3. The site investigation results and the detailed risk assessment (2) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.
 4. A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in (3) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency. Any changes to these components require the express consent of the local planning authority. The scheme shall be implemented as approved.

Reason: *To prevent the pollution of Controlled Waters, notably the underlying Principal groundwater aquifer and the nearby surface water courses and to comply with EM4 and EM5 of the Coventry Local Plan 2016.*

19. No development (including any demolition) within any phase shall take place unless and until a Construction Management Plan (CMP) has been submitted to and approved in writing by the Local Planning Authority. The CMP shall include details of:
 - hours of work;

- hours of deliveries to the site;
- the parking of vehicles of site operatives and visitors during the demolition/construction phase;
- the delivery access point;
- the loading and unloading of plant and materials;
- anticipated size and frequency of vehicles moving to/from the site;
- the storage of plant and materials used in constructing the development;
- the erection and maintenance of a security hoarding including decorative displays and facilities for public viewing where appropriate;
- wheel washing facilities and other measures to ensure that any vehicle, plant or equipment leaving the application site does not carry mud or deposit other materials onto the public highway;
- measures to control the emission of dust and dirt during demolition and construction;
- measures to control the presence of asbestos;
- measures to minimise noise disturbance to neighbouring properties during demolition and construction;
- details of any piling together with details of how any associated vibration will be monitored and controlled; and
- a scheme for recycling / disposing of waste resulting from demolition and construction works.

Thereafter, the approved details within the CMP shall be strictly adhered to throughout the construction period and shall not be amended in any way.

Reason: *The agreement of a Construction Management Plan prior to the commencement of development is fundamental to ensure a satisfactory level of environmental protection; to minimise disturbance to local residents and in the interests of highway safety during the construction process in accordance with Policies AC1 and AC2 of the Coventry Local Plan 2016.*

20. The development hereby permitted shall not be occupied unless and until cycle parking facilities have been provided in full accordance with the approved details. Thereafter those facilities shall remain available for use at all times and shall not be removed or altered in any way.

Reason: *In the interests of encouraging the use of alternative modes of transport with the aim of creating a more sustainable city in accordance with Policies DS3, AC3 and AC4 of the Coventry Local 2016.*

21. The development hereby permitted shall not be occupied unless and until the access to the site, manoeuvring space(s) and car parking area(s) indicated on the approved drawings within that phase have been provided in full accordance with those details and thereafter those areas shall be kept marked out and available for such use at all times.

Reason: *In the interests of highway safety in accordance with the aims and objectives of Policies AC1, AC2 and AC3 of the Coventry Local Plan 2016.*

22. Prior to occupation of the development within any phase details of the footway crossing/bellmouth for the vehicular access shall be submitted to and approved in writing by the Local Planning Authority in the form of a Certificate. Then prior to occupation the footway crossover shall be implemented, retained therein after and kept available for this use at all times.

Reason: *In the interests of highway safety in accordance with the aims and objectives of Policies AC1 and AC2 of the Coventry Local Plan 2016.*

23. The Travel Plan- October 2020 hereby approved shall be implemented in full accordance with the details specified therein. The Travel Plan- October 2020 as approved shall be monitored and reviewed in accordance with the approved Travel Plan- October 2020 targets and shall not be amended in any way unless otherwise agreed in writing with the Local Planning Authority.

Reason: *In the interests of encouraging the use of alternative modes of transport with the aim of creating a more sustainable city in accordance with Policies DS3, AC3 and AC4 of the Coventry Local 2016.*

24. Prior to occupation of phase 1 of the development hereby permitted an HGV routing strategy and trailer parking management plan detailing access routes to and from the site and a strategy for queuing vehicles and trailer parking shall be submitted to approved in writing by the Local Planning Authority. This strategy shall be fully implemented on occupation of the development hereby permitted and thereafter shall only be operated in full accordance with these details.

Reason: *In the interests of highway safety and the free flow of traffic in accordance with the aims and objectives of Policies AC1 and AC2 of the Coventry Local Plan 2016.*

25. Phase 1 use of the development hereby permitted shall not be occupied unless and until details of road directional signage and road markings to Sayer Drive have been submitted to and approved in writing by the Local Planning Authority. The signage and road marking shall be installed in full accordance with the approved details prior to first occupation of phase 1 of the development and thereafter retained.

Reason: *In the interests of highway safety and the free flow of traffic in accordance with the aims and objectives of Policies AC1 and AC2 of the Coventry Local Plan 2016.*

26. The development hereby permitted shall be carried out in accordance with the following approved Dwg:3726-1-105-P5-Phasing Plan unless and alternative phasing plan has been agreed in writing by the Local Planning Authority.

Reason: *To ensure that in the event of the development being carried out on a phased basis, satisfactory access and interim environmental treatment is provided to address the impact in the interests of public safety and amenity in accordance with Policies DS4, DE1, AC1 and AC2 of the Coventry Local Plan 2016.*



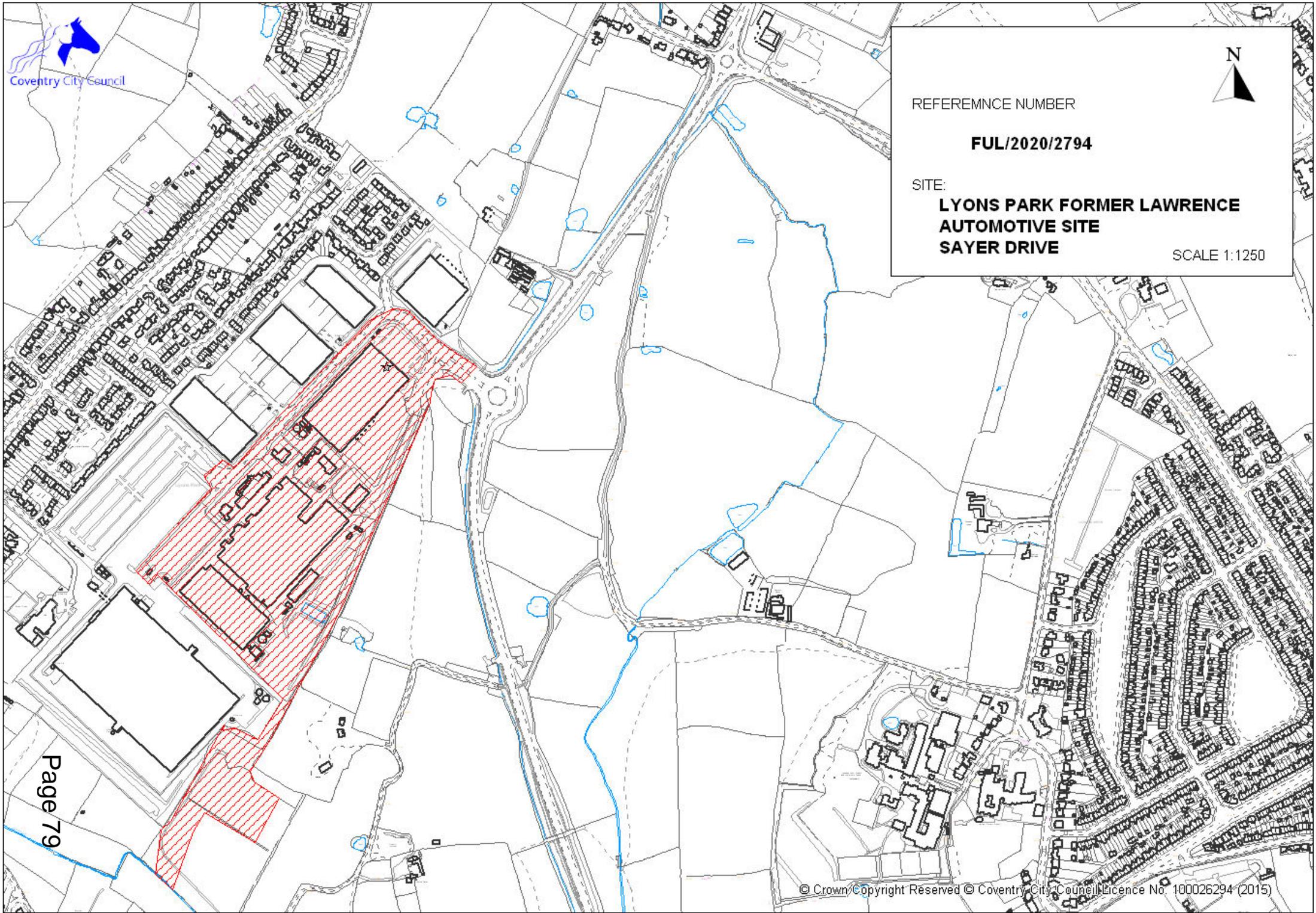
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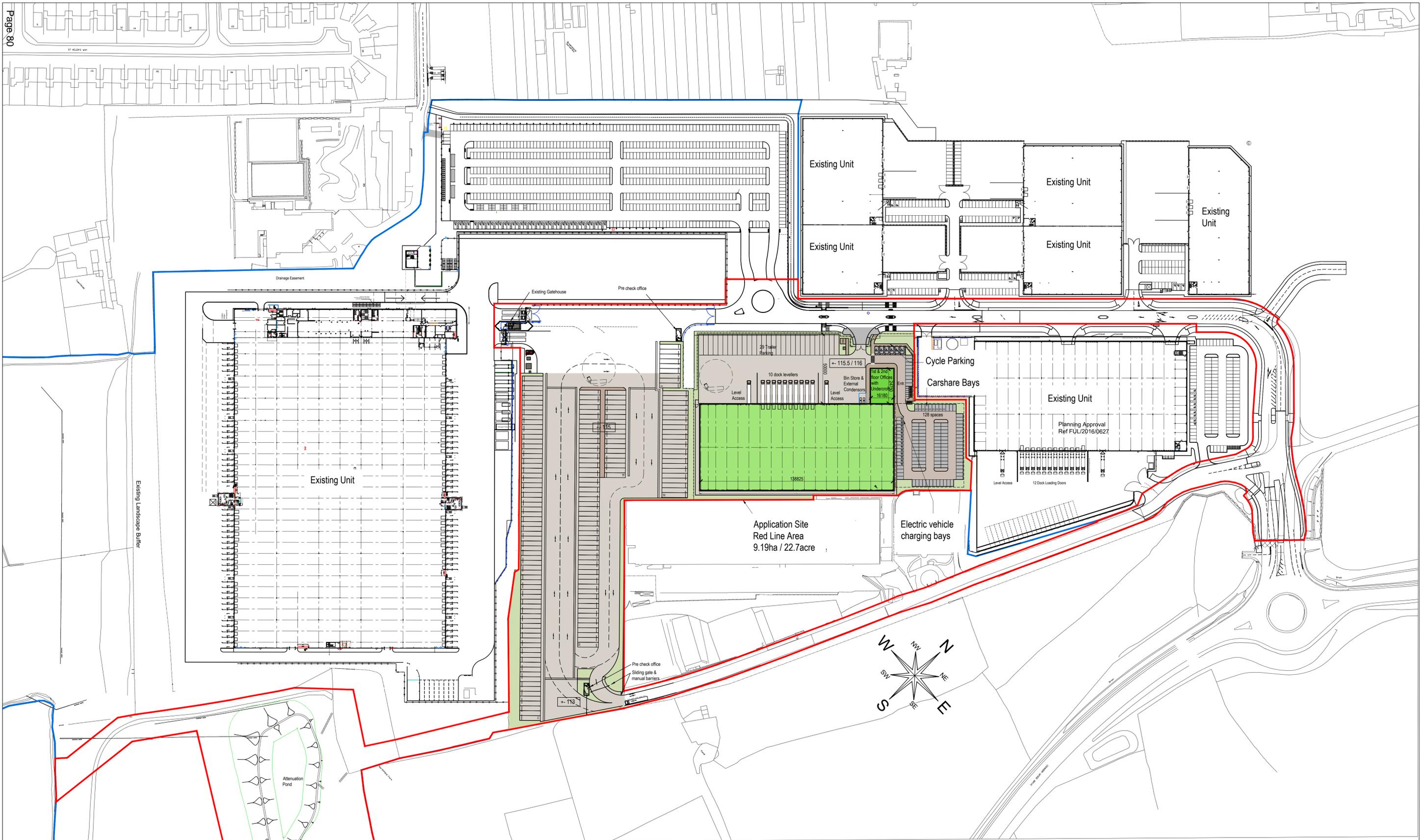
FUL/2020/2794

SITE:

**LYONS PARK FORMER LAWRENCE
AUTOMOTIVE SITE
SAYER DRIVE**

SCALE 1:1250





- Notes:
- Total floorspace figures relate to the proposed warehouse building and exclude Pre-Check Offices for the HGV trailer parking area.
 - Car parking total includes 6 disabled spaces. 2.
 - HGV parking total excludes 173 spaces in the HGV trailer parking area;

Areas Schedule														Parking						
Warehouse GIA sqft	Warehouse GIA sqm	Offices GF Core GIA sqft	Offices GF Core GIA sqm	Future Welfare GF sqft	Future Welfare GF sqm	Offices FF & SF GIA sqft	Offices FF & SF GIA sqm	Total GIA sqft	Total GIA sqm	Total GEA sqft	Total GEA sqm	Pre-Check Office GEA sqft	Pre-Check Office GEA sqm	Cars Parking	HGV Parking	Docks	Level Access	Cycles	PTW's	
92612	8604	710	66	3659	340	8718	810	105699	9820	108478	10078	666	62	128	29	10	2	30	6	
Total	92612	8604	710	3659	340	8718	810	105699	9820	108478	10078	666	62	128	29	10	2	30	6	

Scale: 1/1250

SCALE 1:1250

php architects

PROPOSED DEVELOPMENT LAND
LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
COVENTRY

Issue Purpose:
Drawn by: RG Checked by: RM
Scale @ A1: 1/1250 Date: 09/2020

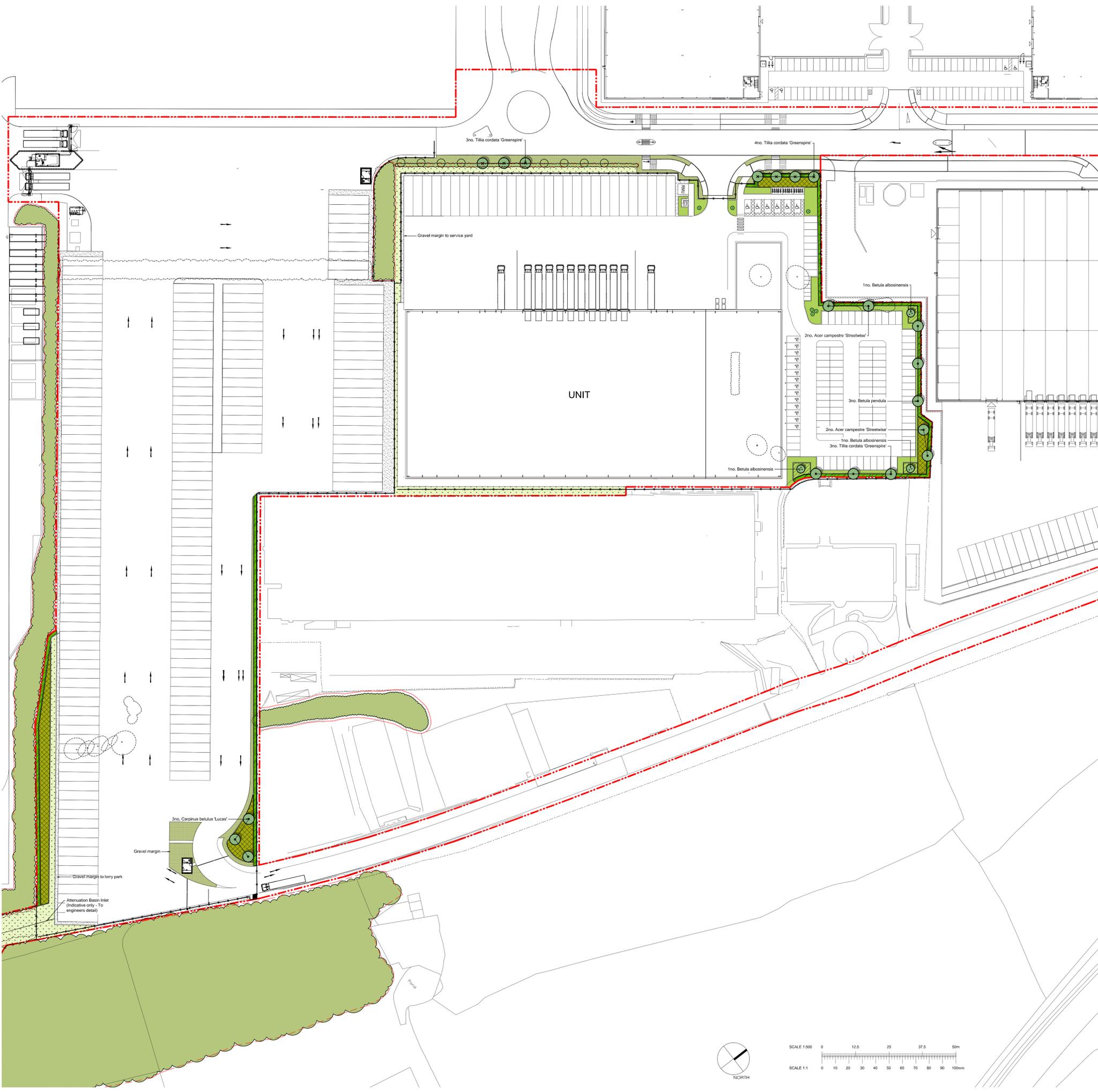
CAD ref: 3726-1 SK248 Dwg no.: 3726-1-100 Rev: P6

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Rev: 28/01/21 rg
19/11/20 rg
07/10/20 rg
06/10/20 rg
02/10/20 rg
28/09/20 rg
Date/Checked

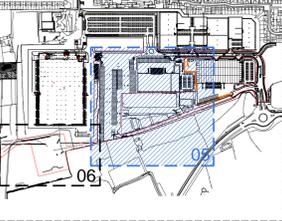
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- EXISTING HEDGEROW AND TREES TO BE RETAINED
Refer to Bca drawing - 2045-20-03-04 Tree Protection Plan for detail.
 - EXISTING HEDGEROW AND TREES TO BE REMOVED
Refer to Bca drawing - 2045-20-03-04 Tree Protection Plan for detail.
 - EXTRA HEAVY STANDARD TREES
(Tree pit size: 1500x1500x900mm backfilled with topsoil)
18-20cm stem girth
4.5-6.5m height
1.8-2.1m clear stem
Rootballed
Acer campestre
Alnus glutinosa
Betula pendula
Tilia cordata 'Greenspire'
Salix alba
 - EXTRA HEAVY STANDARD TREES
(Tree pit size: 1000x1000x750mm backfilled with topsoil)
14-16cm girth
4.25-5.0m height
1.8-2.1m clear stem
Rootballed
Acer campestre 'Streetwise'
Betula pendula
Tilia cordata 'Greenspire'
 - PROPOSED MULTI-STEM TREES
(Tree pit size: 1000x1000x750mm backfilled with topsoil)
3.5-4.0m height
5x transplanted
Rootballed
Betula albosinensis
 - PROPOSED THICKET MIX PLANTING
(300mm depth topsoil)
Transplants planted in groups of 7-15 of the same species on a 1m grid.
E= Evergreen Species.
- | % | Species | Common Name | Supply Size | Age | Root/Pot Size |
|----|----------------------|--------------|-------------|-----|---------------|
| 5 | Acer campestre | Field maple | 450-600mm | 1+1 | OG |
| 15 | Corylus avellana | Hazel | 450-600mm | 1+1 | OG |
| 20 | Cotoneaster Cornubia | | 600-800mm | 3L | (E) |
| 20 | Crataegus monogyna | Hawthorn | 400-600mm | 1+1 | OG |
| 10 | Ilex aquifolium | Holly | 450-600mm | 1+1 | OG (E) |
| 5 | Malus sylvestris | Crab apple | 450-600mm | 1+1 | OG |
| 20 | Prunus laurocerasus | Blackthorn | 400-600mm | 3L | (E) |
| 5 | Viburnum opulus | Gustler rose | 450-600mm | 1+1 | OG |
- Note:
Only shrub species to be used near to drainage runs.
- PROPOSED INDIGENOUS HEDGEROW
(300mm depth of topsoil)
Planted at 450mm centres in a double staggered row. Rows to be 500mm apart.
Transplants planted in groups of 5-6 of the same species. 'Back' row of hedge to be 100% Crataegus. Remaining Crataegus and other species to be used along 'Front' row of hedge. To be clipped and maintained at a height of 1.2m.
E= evergreen species
- | % | Species | Common Name | Size | Age | Root/Pot Size |
|----|-----------------------|-------------|-----------|-----|---------------|
| 5 | Acer campestre | Field maple | 400-600mm | 1+1 | OG |
| 10 | Corylus avellana | Hazel | 400-600mm | 1+1 | OG |
| 60 | Crataegus monogyna | Hawthorn | 400-600mm | 1+1 | OG |
| 5 | Ilex aquifolium (E) | Holly | 400-600mm | 3L | |
| 10 | Ligustrum vulgare (E) | Privet | 400-600mm | 1+1 | OG |
| 10 | Prunus spinosa | Blackthorn | 400-600mm | 1+1 | OG |
- PROPOSED SPECIMEN SHRUBS
(300mm depth of topsoil onto 300mm depth subsoil)
- | Species | Supply Size | Pot Size |
|------------------------------|-------------|----------|
| Araliacanthus canadensis | 900-1200mm | 15L |
| Berberis darwinii | 600-1200mm | 15L |
| Cornus alba 'Elegantissima' | 900-1200mm | 15L |
| Mahonia x media 'Winter Sun' | 900-1200mm | 15L |
| Viburnum x burkwoodii | 900-1200mm | 15L |
- PROPOSED ORNAMENTAL SHRUB PLANTING
(300mm depth of topsoil onto 300mm depth subsoil)
Ultimate plant height is above 1m.
- | Species | Supply Size | Pot Size | Spacing |
|---------------------------------|-------------|----------|-----------|
| Berberis thunbergii | 300-400mm | 3L | 600mm c/s |
| Berberis x ottawensis 'Superta' | 400-600mm | 3L | 600mm c/s |
| Buddlia davidii 'Empire Blue' | 400-600mm | 3L | 750mm c/s |
| Ceanothus 'Blue Mound' | 300-400mm | 3L | 600mm c/s |
| Cornus alba 'Sibirica' | 400-600mm | 3L | 600mm c/s |
| Corylus avellana | 600-800mm | OG | 600mm c/s |
| Cotoneaster franchetii | 400-600mm | 3L | 600mm c/s |
| Ilex aquifolium | 400-600mm | 3L | 750mm c/s |
| Mahonia x 'Winter Sun' | 300-400mm | 3L | 600mm c/s |
| Photinia Fraseri 'Red Robin' | 400-600mm | 3L | 750mm c/s |
| Pyracantha 'Sashy Orange' | 400-600mm | 3L | 600mm c/s |
| Spiraea nipponica 'Snowmound' | 400-600mm | 3L | 600mm c/s |
| Spiraea x arguta | 300-400mm | 3L | 600mm c/s |
| Sambucus nigra 'Aurea' | 600-800mm | 3L | 750mm c/s |
| Viburnum tinus 'Eve Price' | 200-300mm | 3L | 750mm c/s |
| Viburnum x burkwoodii | 300-400mm | 3L | 600mm c/s |
- PROPOSED LOW GROUNDCOVER PLANTING
(300mm depth of topsoil onto 300mm depth subsoil)
Ultimate plant height is below 1m.
- | Species | Supply Size | Pot Size | Spacing |
|------------------------------------|-------------|----------|-----------|
| Cornus stolonifera 'Hedgehog' | 200-300mm | 3L | 450mm c/s |
| Cotoneaster dammeri 'Oakwood' | 300-400mm | 3L | 500mm c/s |
| Escallonia 'Apple Blossom' | 400-600mm | 3L | 500mm c/s |
| Euonymus fortunei 'Emerald Gaiety' | 200-300mm | 3L | 450mm c/s |
| Gonolobus yulii | 300-400mm | 3L | 450mm c/s |
| Gentiana Johnsonii 'Blue' | 200-300mm | 2L | 400mm c/s |
| Hebe albicans 'Red Edge' | 200-300mm | 3L | 500mm c/s |
| Hedera helix 'Hibernica' | 400-600mm | 2L | 400mm c/s |
| Lonicera sibirica 'Magnifica' | 300-400mm | 3L | 600mm c/s |
| Pachysandra terminalis | 150-200mm | 2L | 400mm c/s |
| Penstemon alpinus 'Dorset Blue' | 200-300mm | 2L | 450mm c/s |
| Potentilla dahurica 'Abbotswood' | 200-300mm | 3L | 500mm c/s |
| Prunus laurocerasus 'Oro Luyker' | 300-400mm | 3L | 600mm c/s |
| Rosa 'Max Graf' | 400-600mm | 2L | 450mm c/s |
| Rubus 'Betsy Booth' | 300-400mm | 2L | 500mm c/s |
| Spiraea japonica 'Little Princess' | 200-300mm | 3L | 600mm c/s |
| Stephanandra incisa 'Chippa' | 200-300mm | 2L | 500mm c/s |
| Viburnum davidii | 200-300mm | 3L | 450mm c/s |
| Vincia minor 'Atropurpurea' | 200-300mm | 2L | 450mm c/s |
| Wegelia florida 'Folies Purpureas' | 400-600mm | 3L | 600mm c/s |
- PROPOSED CLOSE MOWN GRASS AREAS
(150mm depth of topsoil onto 150mm depth subsoil)
DLF trifolium pro master 120 slowgrowth to be sown in accordance with manufacturers specification. Or similar approved.
 - PROPOSED WILDFLOWER GRASS SEED AREAS
(Cultivated subsoil to a depth of 300mm)
Emorsgate Basic General Purpose Meadow mixture EM1 to be sown in accordance with manufacturers specification.
 - SLOPE
 - FENCE
 - GRAVEL MARGIN
 - SITE BOUNDARY



NOTES

DRAWING LOCATION PLAN



Refer to drawing 2045-20-06E for detail

REV H	Finalised adjusted	22-02-21 HC
REV G	Drawing annotation revised.	01-02-21 HC
REV F	Updated to show revised attenuation pond design	28-01-21 HC
REV E	Updated to suit latest architects & engineering plans	25-01-21 HC
REV D	Civils storage location & landscaping updated to new layout	07-12-20 HC
REV C	Amended to updated architects layout	04-12-20 HC
REV B	Attention following client comments	19-11-20 MAB
REV A	Minor layout update	06-11-20 MAB
REV NOTE	DATE AUTH	

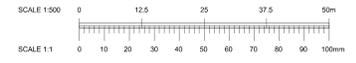
BCA BARRY CHINN
associates
Landscape Architects

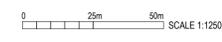
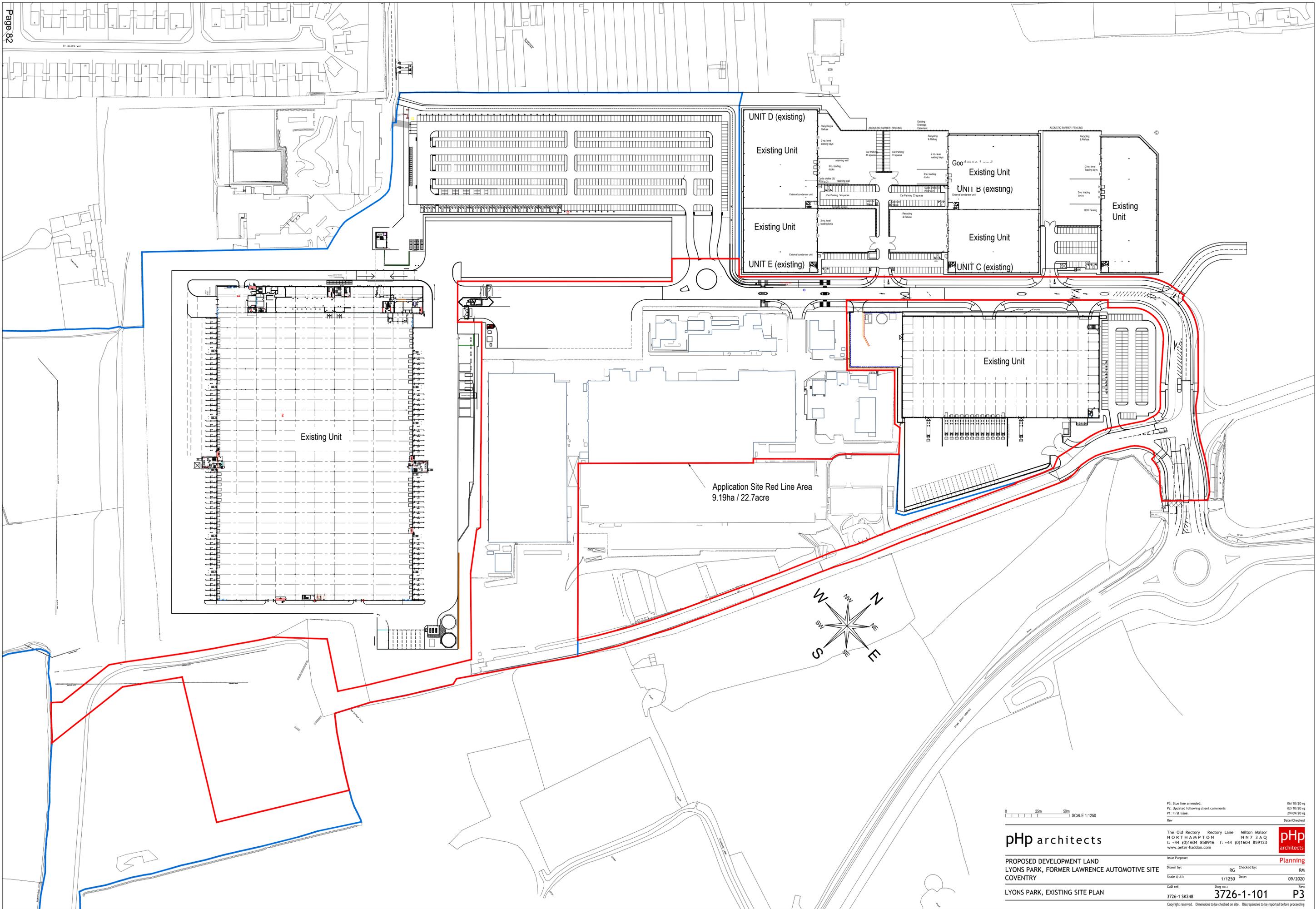
CLIENT
Goodman

PROJECT
**LAWRENCE AUTOMOTIVE LAND,
LYONS PARK.**

DRAWING
**LANDSCAPE CONCEPT PLAN
(Sheet 1 of 2)**

CONTRACT	2045-20	DRG NO.	
DATE	22-02-20	DRAWN	HC
ISSUE	Comment	CHECKED	MB
SCALE	1:500	ORIG SHEET	AD
CAD FILE	2045-20-05-06 Landscape Concept.dwg	REV	H





P2: Blue line amended. 06/10/20 rg
 P2: Updated following client comments 02/10/20 rg
 P1: First Issue. 29/09/20 rg
 Rev Date/Checked

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PROPOSED DEVELOPMENT LAND
 LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
 COVENTRY

Issue Purpose:
 Drawn by: RG Checked by: RM
 Scale @ A1: 1/1250 Date: 09/2020

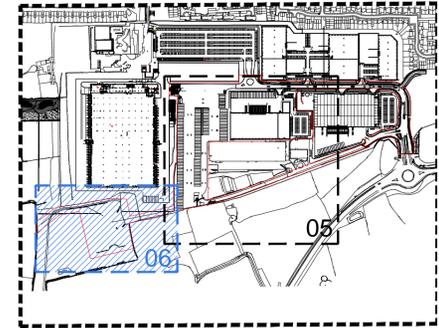
LYONS PARK, EXISTING SITE PLAN

CAD ref: 3726-1-SK248 Dwg no.: 3726-1-101 Rev: P3

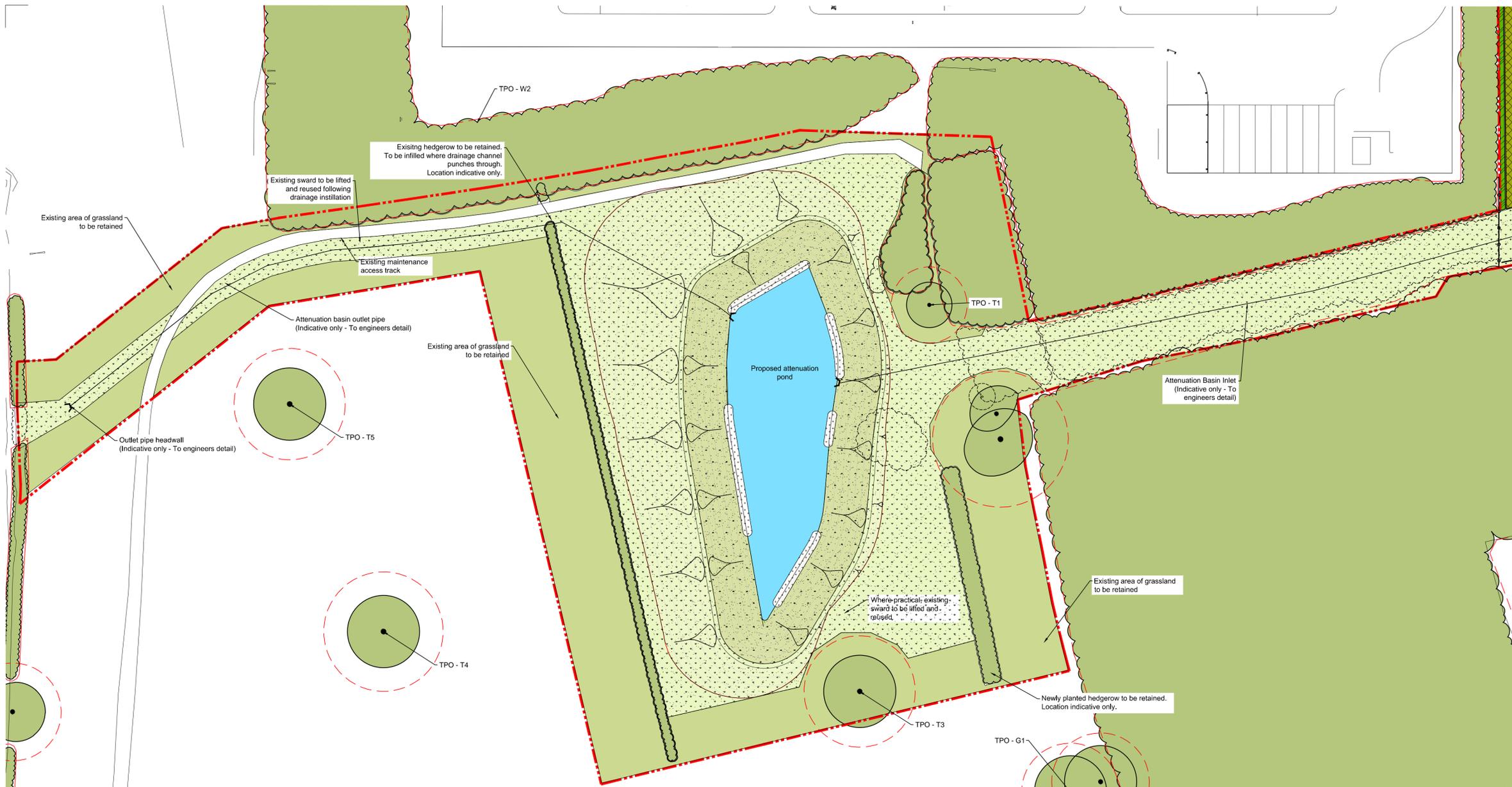
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NOTES

DRAWING LOCATION PLAN

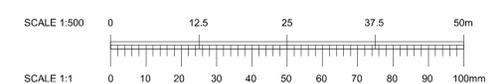


Refer to drawing 2045-20-05F for detail



KEY

-  EXISTING HEDGEROW AND TREES TO BE RETAINED
Refer to Bca drawing - 2045-20-03+04 Tree Protection Plan for detail.
-  EXISTING HEDGEROW AND TREES TO BE REMOVED
Refer to Bca drawing - 2045-20-03+04 Tree Protection Plan for detail.
-  PROPOSED WILDFLOWER GRASS SEED AREAS
(Cultivated subsoil to a depth of 300mm)
Emorsgate Basic General Purpose Meadow mixture EM1 to be sown in accordance with manufacturers specification.
-  PROPOSED WET WILDFLOWER GRASS SEED AREAS
(150mm depth of topsoil onto 150mm depth subsoil)
Emorsgate Pond Edge mixture EP1 to be sown in accordance with manufacturers specification.
-  EXISTING GRASSLAND TO BE RETAINED
-  PROPOSED AQUATIC PLANTING
(150mm depth of topsoil onto 150mm depth subsoil)
-  SLOPE
-  SITE BOUNDARY



Rev F Drawing annotation revised.	01/02/21 HC
Rev E Updated to show revised attenuation pond design	28/01/21 HC
Rev D Updated to reflect ecologist recommendations	25/01/21 HC
Rev C Amended to suit updated architects layout	04/12/20 MB
Rev B Alteration following client comments	10/11/20 MB
Rev A Newly planted hedgerows added & design amended to suit	27/10/20 HC
REV NOTE	DATE AUTH

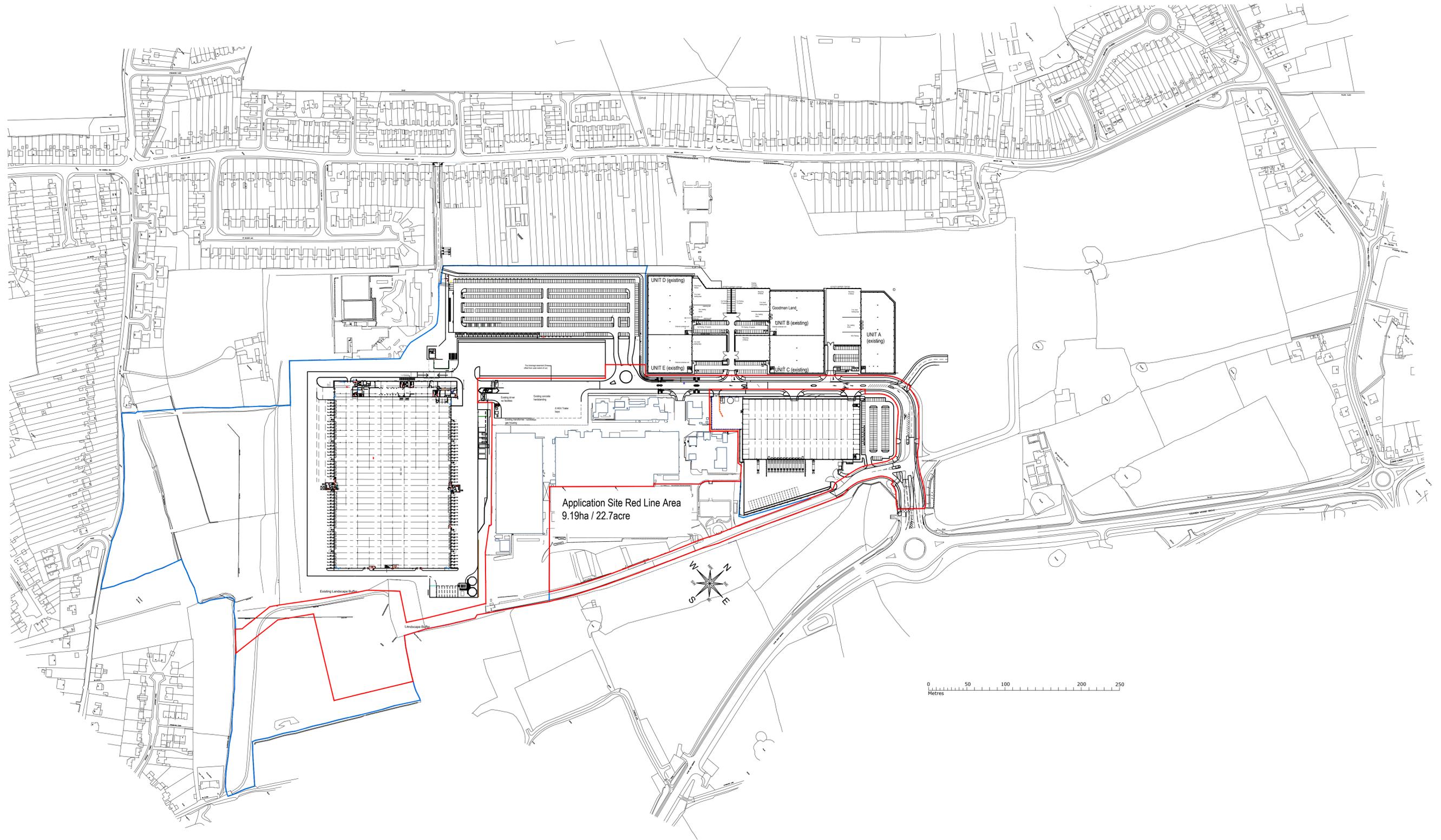


CLIENT
Goodman

PROJECT
**LAWRENCE AUTOMOTIVE LAND,
LYONS PARK**

DRAWING
**LANDSCAPE CONCEPT PLAN
(Sheet 2 of 2)**

CONTRACT	2045-20	DRG NO.	06
DATE	20-10-20	DRAWN	
ISSUE	Comment	CHECKED	MB
SCALE	1:500	ORIG SHEET	A1
CAD FILE	2045-20-05+06 Landscape Concept.dwg		REV F

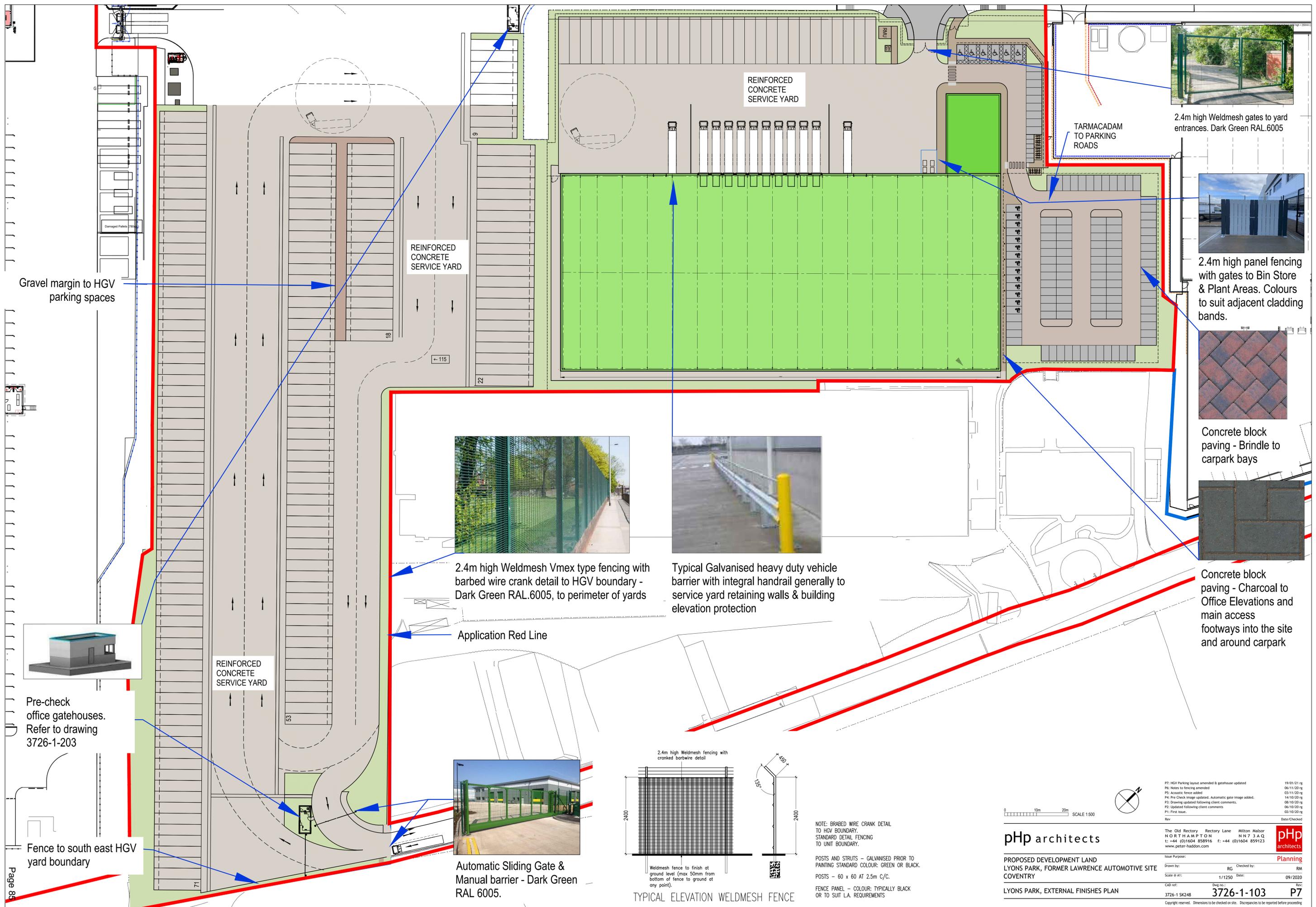


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PROPOSED DEVELOPMENT LAND
 LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
 COVENTRY

LYONS PARK, LOCATION PLAN

P2: Blue Line amended	06/10/20 rg
P2: Updated following client comments	02/10/20 rg
P1: First Issue	29/09/20 rg
Rev	Date/Checked
The Old Rectory Rectory Lane Milton Malsor NORTHAMPTON NN7 3AQ t: +44 (0)1604 858916 f: +44 (0)1604 859123 www.peter-haddon.com	
Issue Purpose:	Planning
Drawn by: RG	Checked by: RM
Scale @ A1: 1/2500	Date: 09/2020
CAD ref:	Rev:
3726-1 SK248	3726-1-102 P3
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2.4m high Weldmesh gates to yard entrances. Dark Green RAL.6005



2.4m high panel fencing with gates to Bin Store & Plant Areas. Colours to suit adjacent cladding bands.



Concrete block paving - Brindle to carpark bays



Concrete block paving - Charcoal to Office Elevations and main access footways into the site and around carpark



2.4m high Weldmesh Vmex type fencing with barbed wire crank detail to HGV boundary - Dark Green RAL.6005, to perimeter of yards



Typical Galvanised heavy duty vehicle barrier with integral handrail generally to service yard retaining walls & building elevation protection

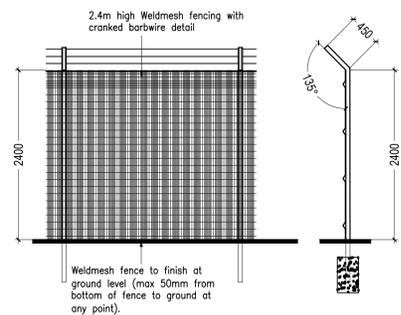
Application Red Line



Pre-check office gatehouses. Refer to drawing 3726-1-203



Automatic Sliding Gate & Manual barrier - Dark Green RAL 6005.



TYPICAL ELEVATION WELDMESH FENCE

NOTE: CRANKED WIRE CRANK DETAIL TO HGV BOUNDARY.
STANDARD DETAIL FENCING TO UNIT BOUNDARY.
POSTS AND STRUTS - GALVANISED PRIOR TO PAINTING STANDARD COLOUR: GREEN OR BLACK.
POSTS - 60 x 60 AT 2.5m C/C.
FENCE PANEL - COLOUR: TYPICALLY BLACK OR TO SUIT L.A. REQUIREMENTS

0 10m 20m SCALE 1:500

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PROPOSED DEVELOPMENT LAND
LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
COVENTRY

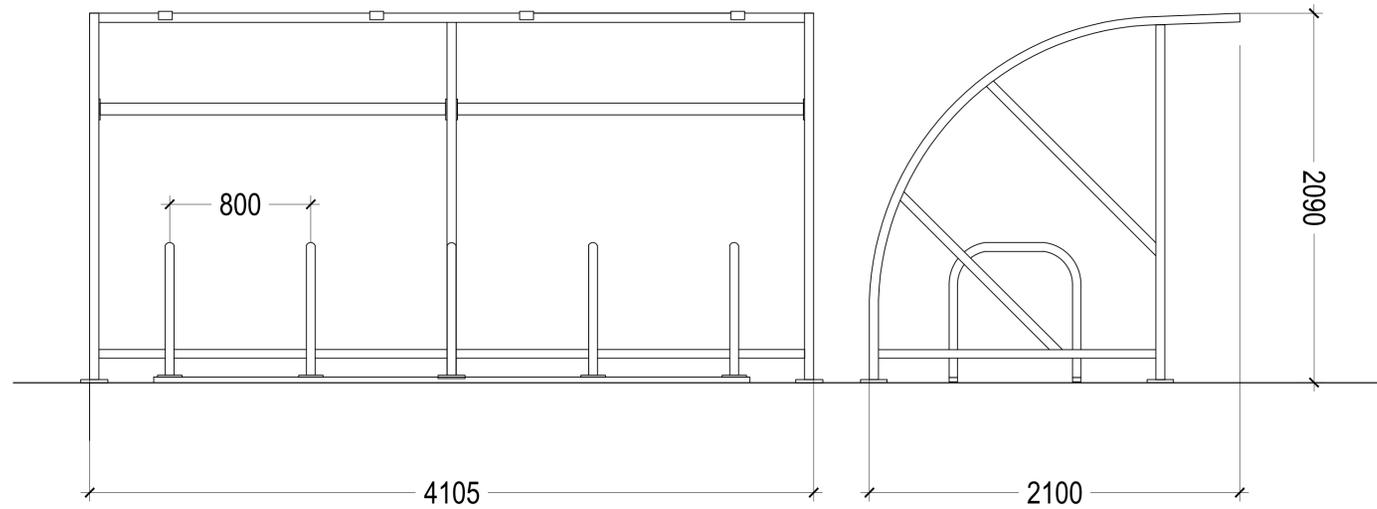
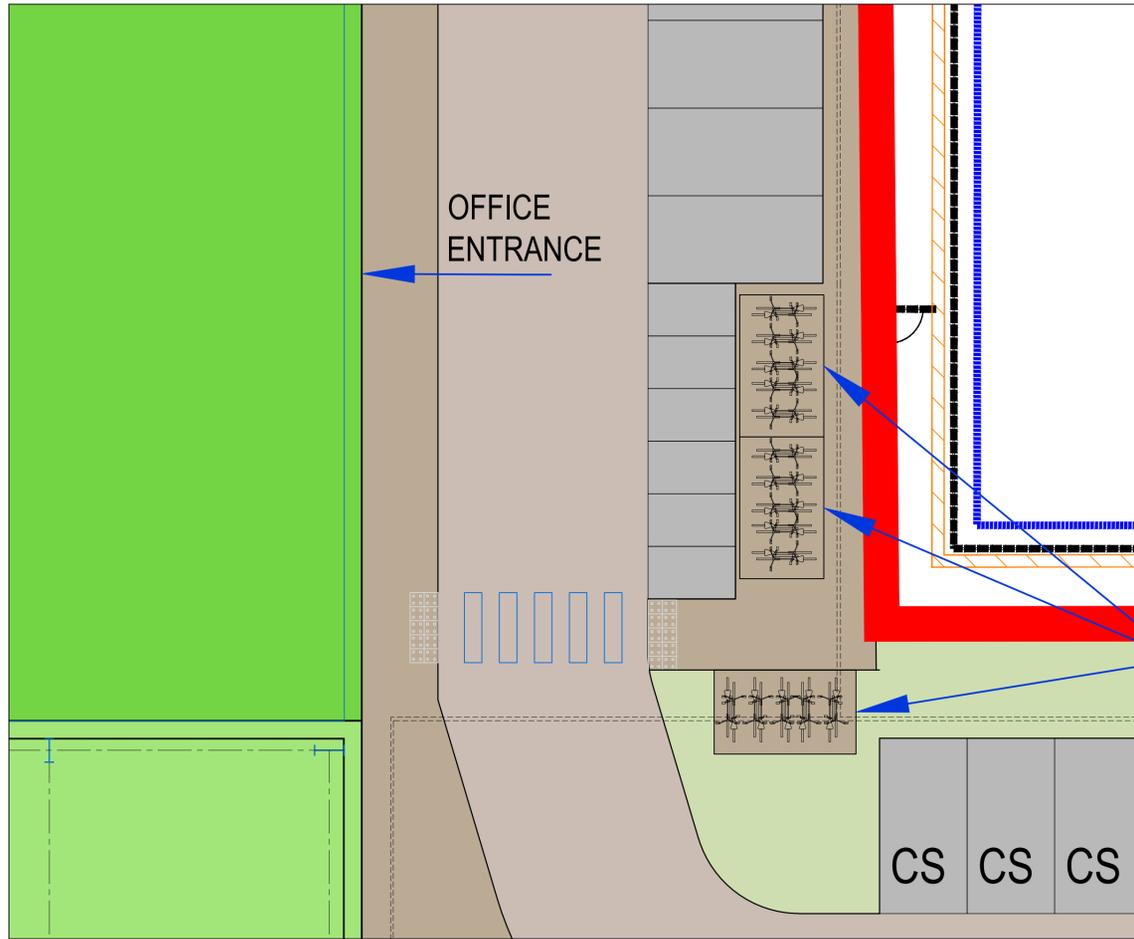
LYONS PARK, EXTERNAL FINISHES PLAN

3726-1-SK248 3726-1-103 P7

19/01/21 rg
06/11/20 rg
03/11/20 rg
14/10/20 rg
08/10/20 rg
06/10/20 rg
02/10/20 rg

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Issue Purpose:
Drawn by: RG Checked by: RM
Scale @ A1: 1/1250 Date: 09/2020
CAD ref: Dwg no.:
3726-1-SK248 3726-1-103 P7
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3no cycle shelters

TYPICAL ELEVATION CYCLE SHELTER



General Specification

- Clear Polycarbonate curvel panels / end panels
- Adjustable feet
- Modular galvanised 50x50 RHS steel box frame
- Integral hoops
- Colour TBC
- Approx. 4m (L) x 2.1 (D) x 2.1m (H)
- 3no. racks to provide 30 cycle spaces
- Covered cycle shelter(s) with galzanised cross strut cycle hoops to be provided in accordance with LPA & BREEAM requirements. (at least 1 cycle locking space per 10 No. staff.)



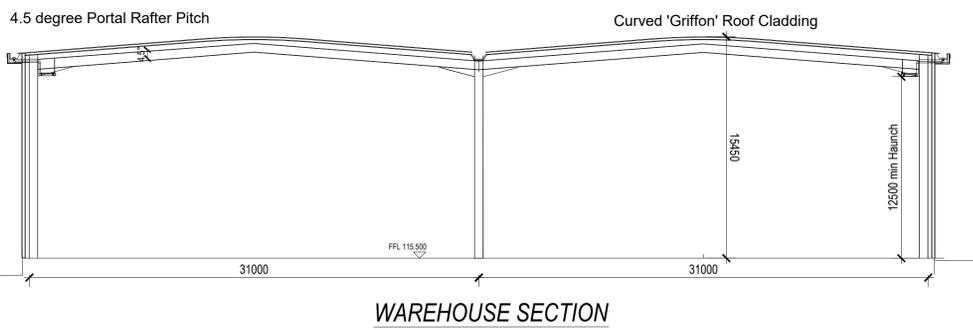
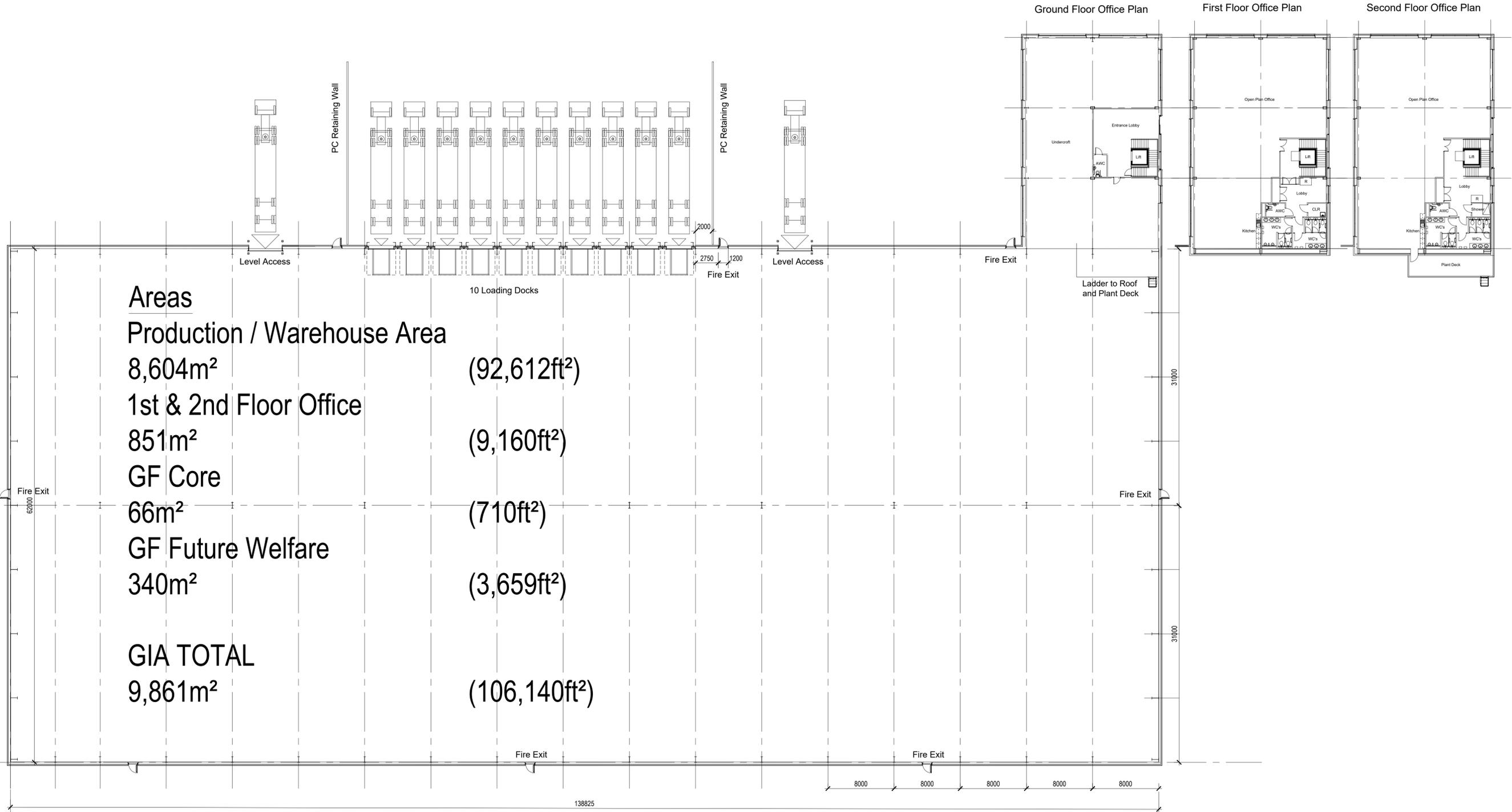
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PROPOSED DEVELOPMENT LAND
LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
COVENTRY

LYONS PARK, CYCLE SHELTER

P2: Notes added to Cycle Storage	06/11/20 rg
P1: First Issue.	08/10/20 rg
Rev	Date/Checked
The Old Rectory Rectory Lane Milton Malsor NORTHAMPTON NN7 3AQ t: +44 (0)1604 858916 f: +44 (0)1604 859123 www.peter-haddon.com	php architects
Issue Purpose:	Planning
Drawn by: RG	Checked by: RM
Scale @ A1: 1/20	Date: 09/2020
CAD ref:	Rev:
3726-1 SK248	3726-1-106 P2

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P5: FFL level added
 P4: Plant Deck ladder relocated.
 P3: Plans updated to show plant deck and roof access
 P2: Updated following client comments
 P1: First Issue

16/10/20 rg
 07/10/20 rg
 06/10/20 rg
 02/10/20 rg
 28/09/20 rg

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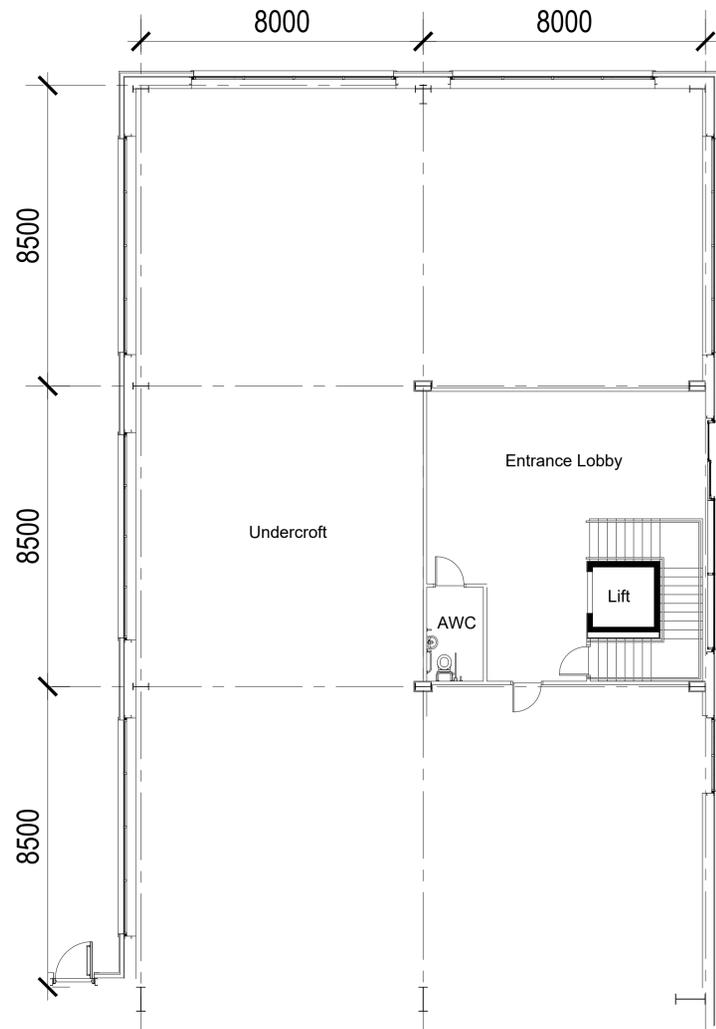
Issue Purpose: **Planning**
 Drawn by: RG Checked by: RM
 Scale @ A1: 1/250 Date: 09/2020
 CAD ref: 3726 Draw no.: **3726-1-200** Rev: **P5**

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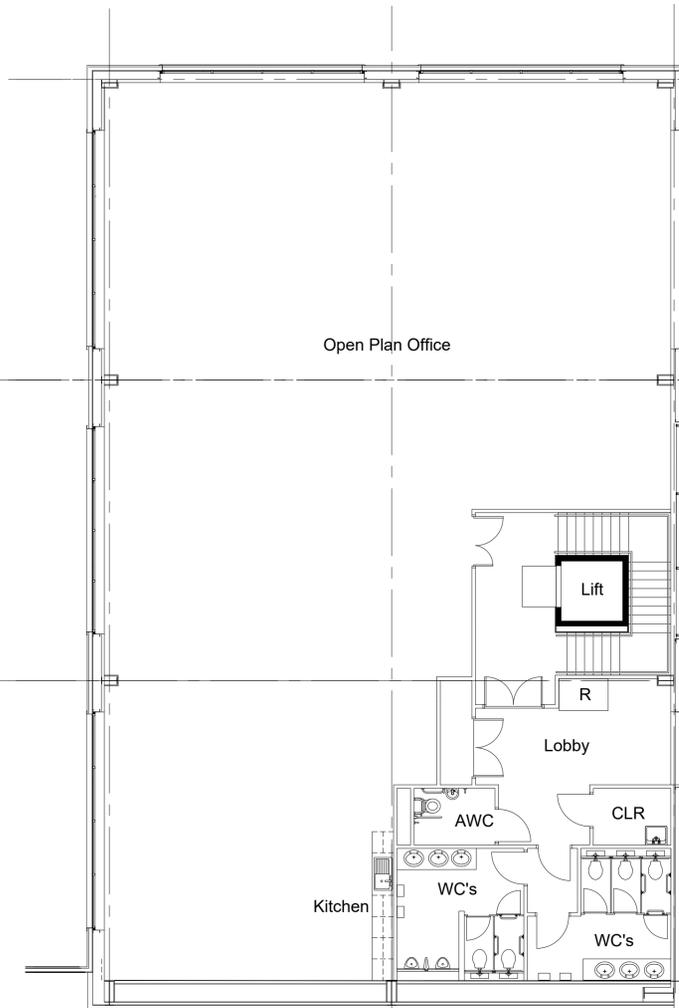
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PROPOSED DEVELOPMENT LAND
 LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
 COVENTRY
 LYONS PARK, UNIT PLAN

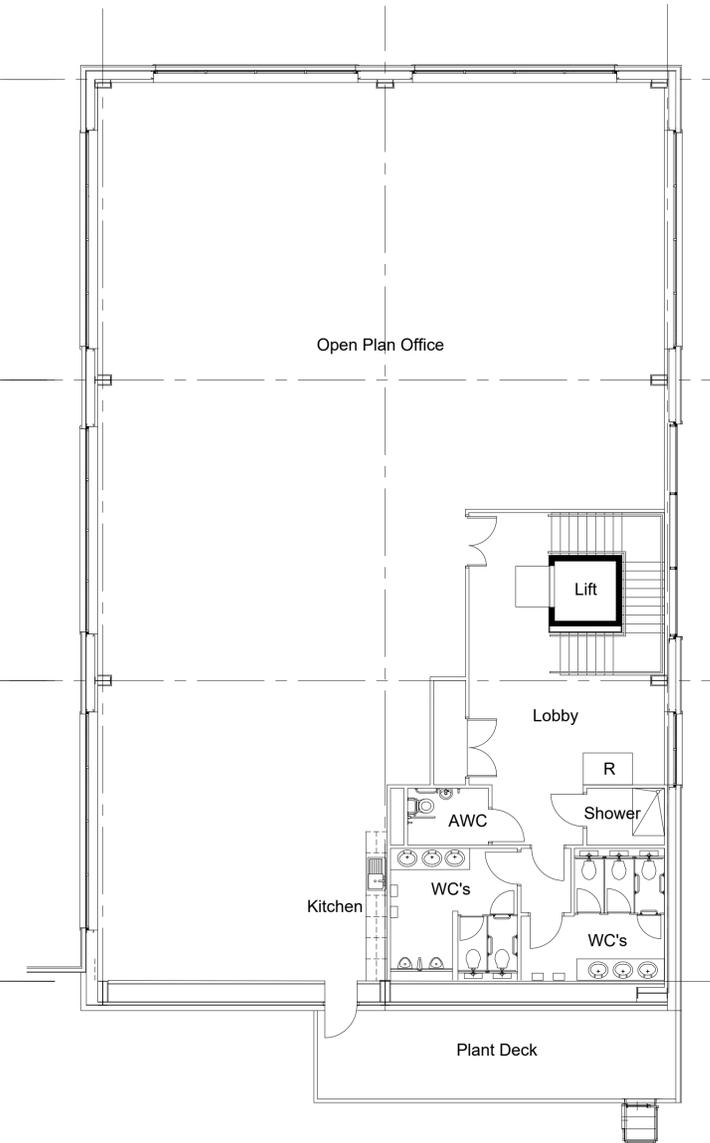
Ground Floor Office Plan



First Floor Office Plan



Second Floor Office Plan



0 2.5m 5m SCALE 1:100

P4: Plant Deck ladder relocated. 07/10/20 rg
 P3: Drawings updated to show plant deck and riser. 06/10/20 rg
 P2: Updated following client comments. 02/10/20 rg
 P1: First Issue 28/09/20 rg

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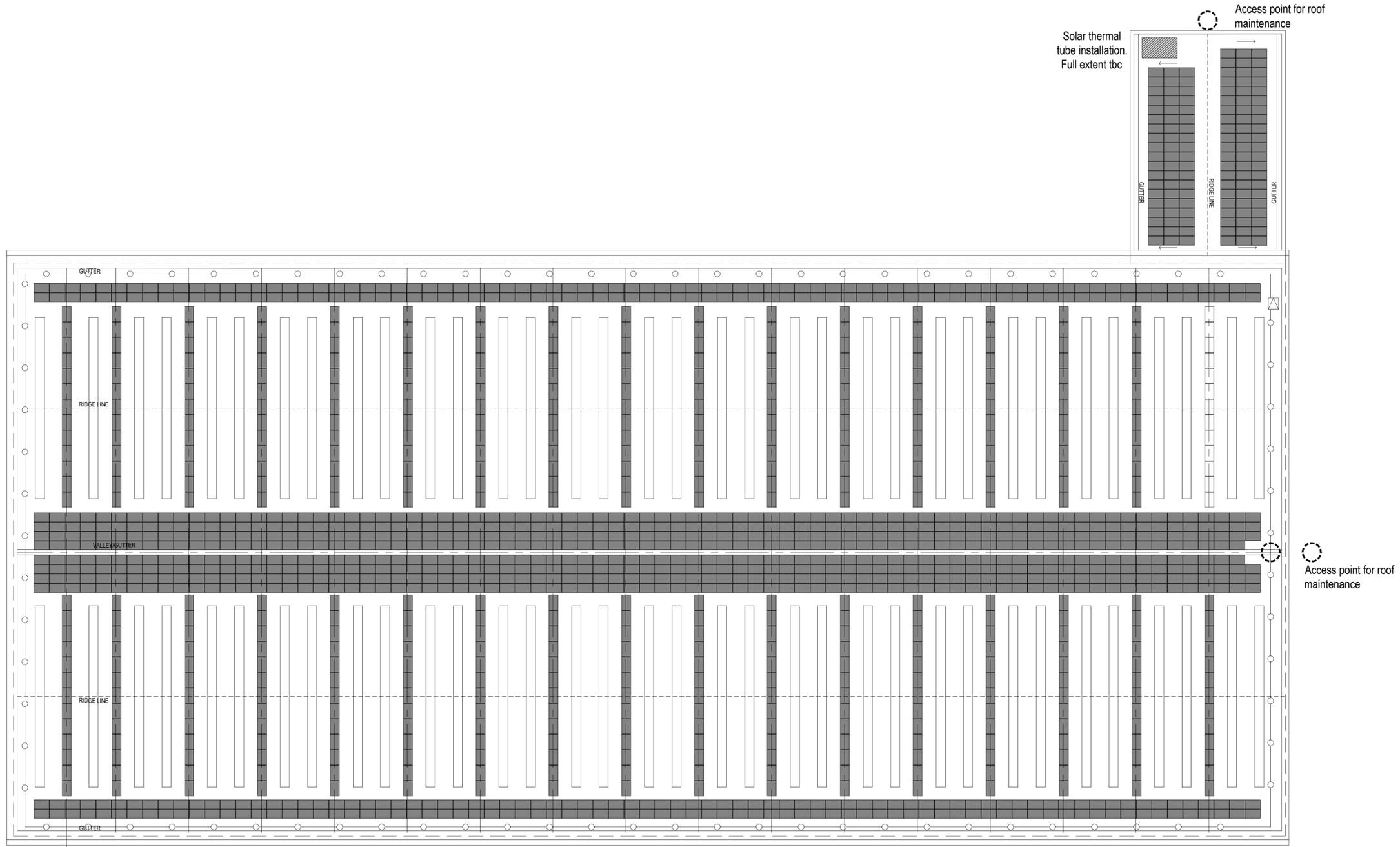
PROPOSED DEVELOPMENT LAND
 LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
 COVENTRY

Issue Purpose:
 Drawn by: RG Checked by: RM
 Scale @ A1: 1/100 Date: 09/2020

LYONS PARK, OFFICE PLANS

CAD ref: 3726 Dwg no.: 3726-1-201 Rev: P4

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0 5m 10m SCALE 1:250

P6: Note removed as requested. 04/11/20 rg
 P5: PV added to plan. 15/10/20 rg
 P4: Ladder hatch to roof relocated. Fall restraint added. 07/10/20 rg
 P3: Roof Plan updated. 06/10/20 rg
 P2: Updated following client comments. 02/10/20 rg
 P1: First Issue. 28/09/20 rg

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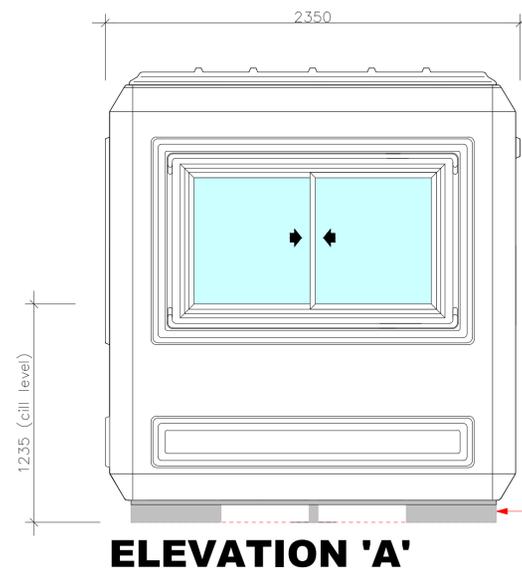
PROPOSED DEVELOPMENT LAND
LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
COVENTRY

Issue Purpose:
 Drawn by: RG Checked by: RM
 Scale @ A1: 1/250 Date: 09/2020

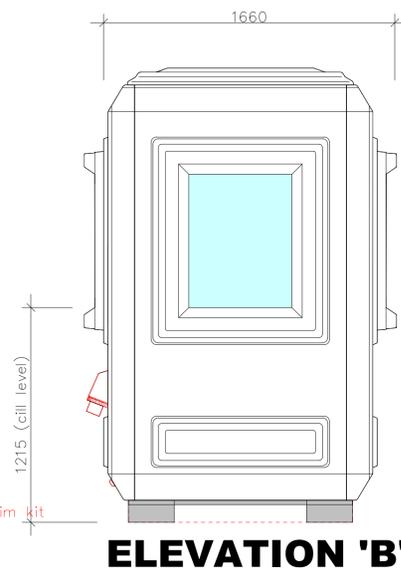
LYONS PARK, ROOF PLAN

CAD ref: 3726 Dwg no.: 3726-1-202 Rev: P5

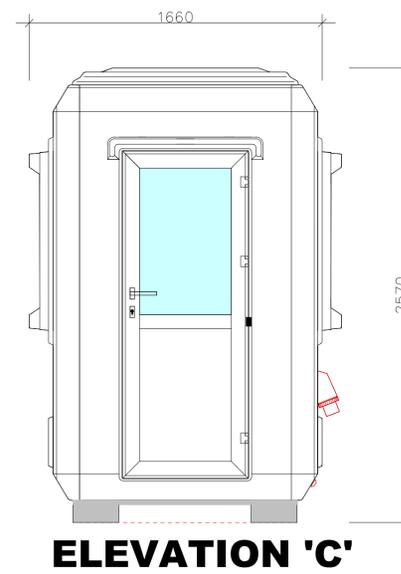
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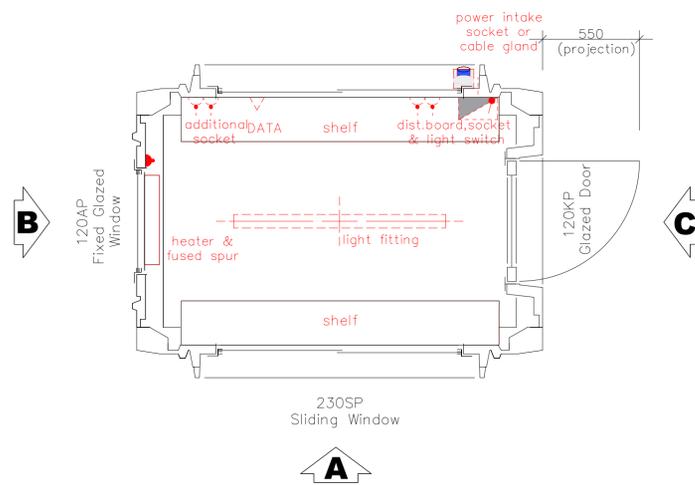
ELEVATION 'A'



ELEVATION 'B'



ELEVATION 'C'



PLAN VIEW

PRODUCT INFORMATION

DESCRIPTION :
Fixed or free standing kiosk c/w fork lift points.

MATERIAL :
Panels - GRP panels with fire retardent foam and resin.
Floor - Anti-slip plywood 18mm thick, black finish.
Subframe - Mild steel with galvanised finish.

COLOUR :
Standard 'White'.

DIMENSIONS :
All dimensions are in mm unless otherwise stated.

OPTIONAL EXTRAS :
(see plan view for pre-determined layouts in red).
Electrical kit (light & switch, double socket).
Additional double socket.
750w convector heater.
250mm Deep shelf.
Data socket unwired.
External power intake socket or cable gland.
Plinth trim kit (installed at site by others).

ELECTRICAL INSTALLATION:
All electrical installation work undertaken as part of the supply of the unit is in accordance with BS 7671: 2018: Requirements for Electrical Installations. Positions of electrical items are for illustrative purposes only, with final positions and requirements for additional connection units, light switches etc. to be determined by the Electrical Supervisor during assembly of the unit, to ensure the installation complies with the above standard and the requirements of the IET Wiring Regulations Eighteenth Edition.

INSTALLATION :
Non permanent - a firm level surface.
Permanent - Base to be a minimum of 2200 x 1400 x 125mm thick concrete onto well consolidate hardcore. Base to be smooth (float finish), flat, level, square and slightly raised above surrounding ground level. Drill four 12 x 90mm holes for use with expanding ground anchor bolts, locations pre-determined within subframe.

Note: 2no Pre-check offices required, see site plan for locations and orientations



P2: Drawing updated following client comments 14/10/20 RG
P1: First Issue 05/10/20 JM
Rev Date/Checked

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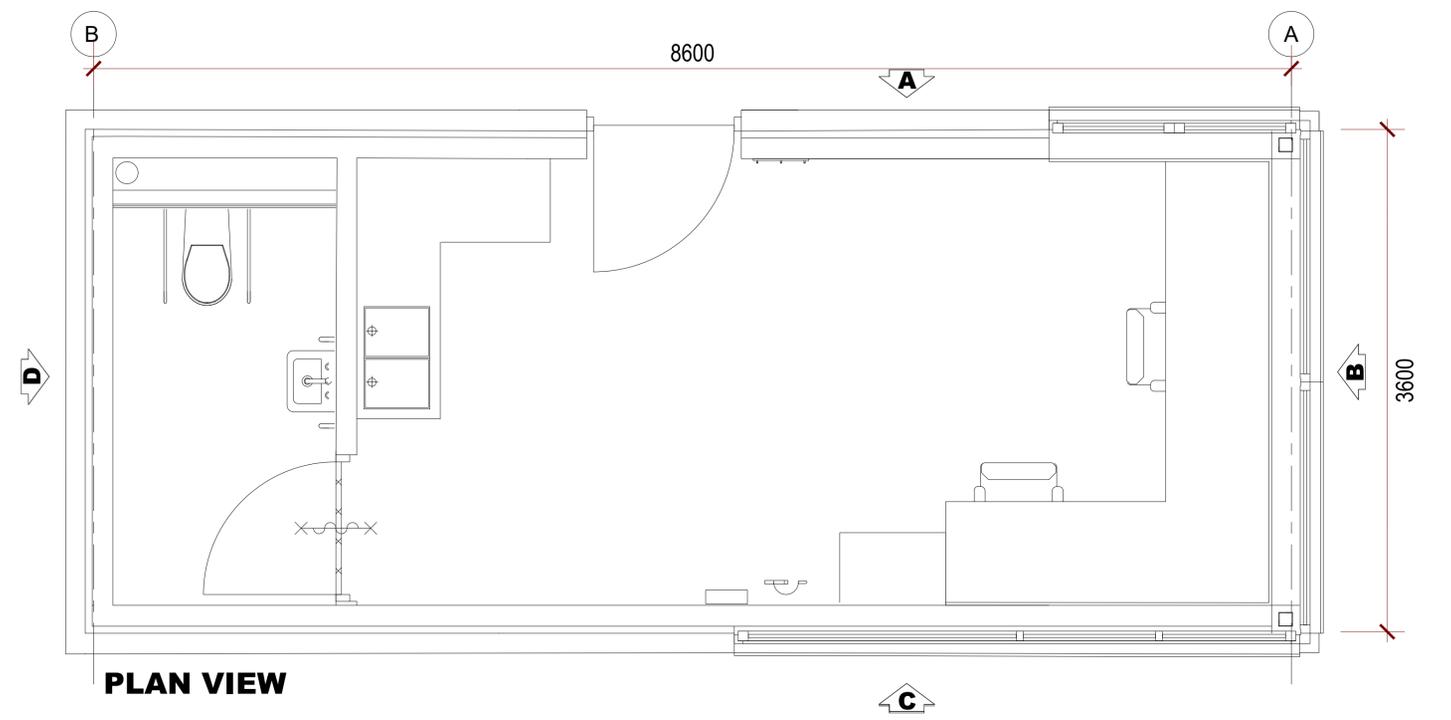
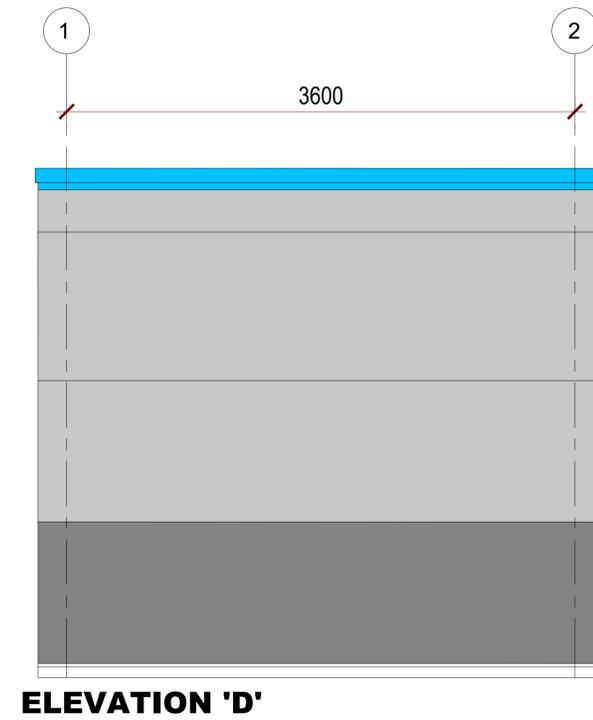
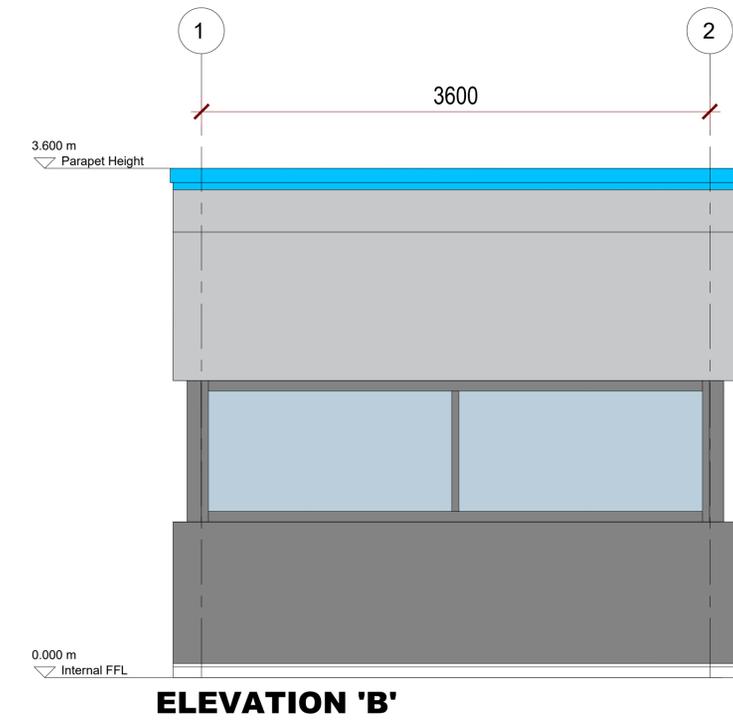
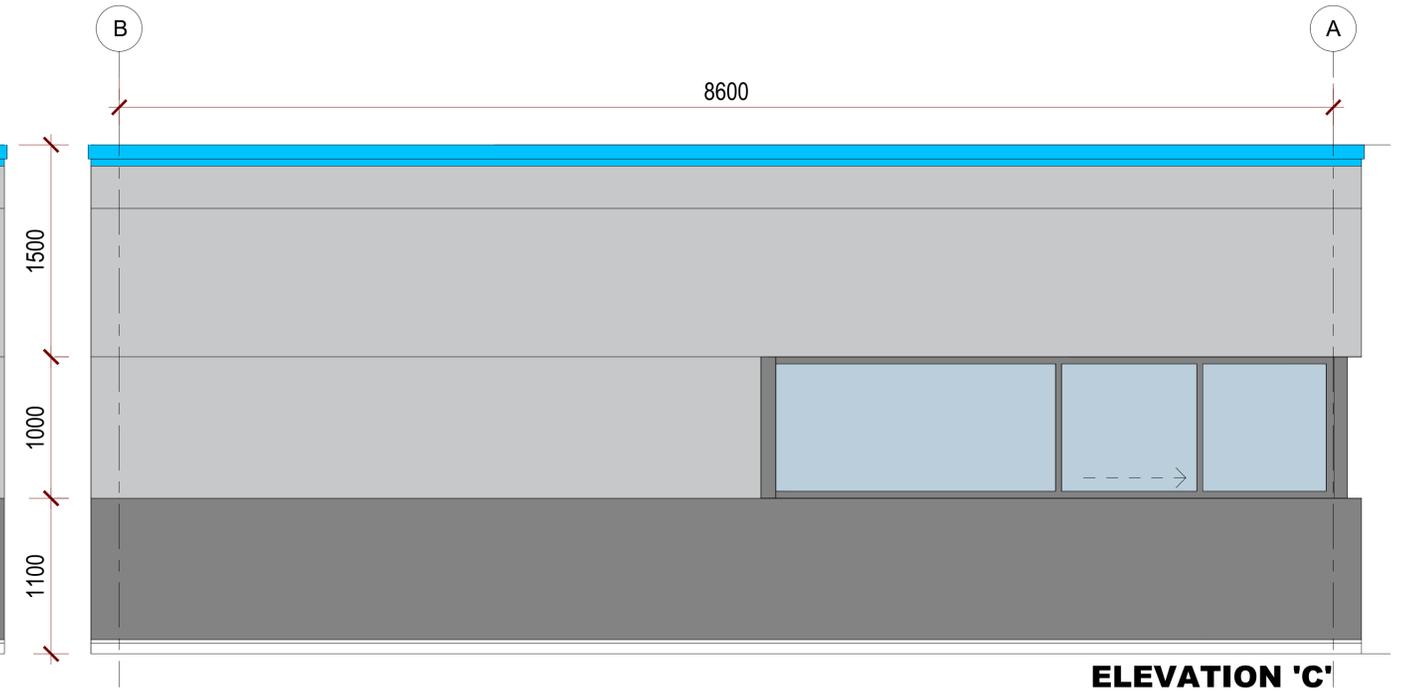
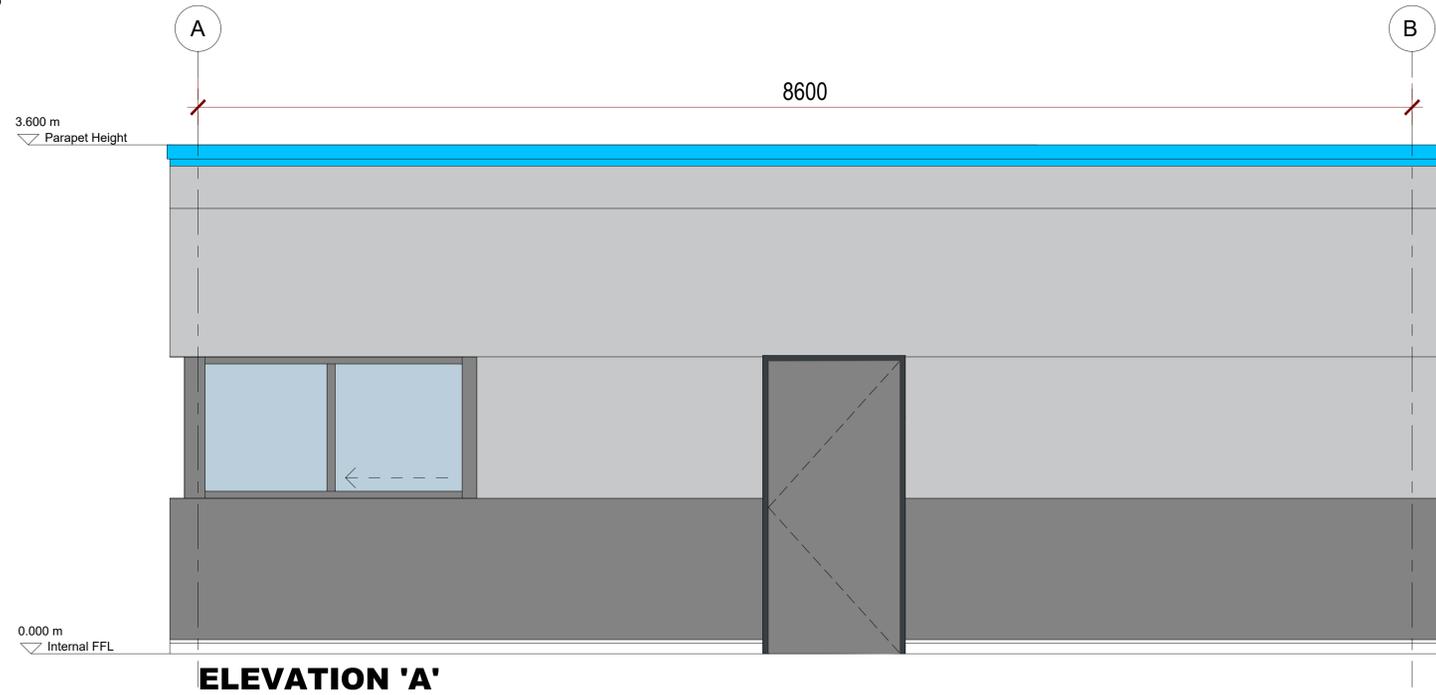
PROPOSED DEVELOPMENT LAND
LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
COVENTRY

Issue Purpose:
Drawn by: JM Checked by: KI
Scale @ A1: 1:20 Date: 09/2020

PRE-CHECK OFFICE PLANS & ELEVATIONS

CAD ref: 3726 Dwg no.: 3726-1-203 Rev: P2

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PRODUCT INFORMATION

Prefabricated units sited on a suitable concrete base.

MATERIAL :
 Panels - Insulated composite cladding laid horizontally - Merlin Grey RAL 180 40 05
 Plinth - Insulated composite cladding laid horizontally - RAL 7016
 Doors and Windows - Anthracite RAL 7016
 Fascia - PPC aluminium RAL 5012
 Floor - Ceramic tiled anti-slip floor to WC. Vinyl floor and skirting to office area
 Subframe - Concrete base

DIMENSIONS :
 All dimensions are in mm unless otherwise stated.

ELECTRICAL INSTALLATION:
 LED light fixtures:
 Exterior:
 Interior: 10 lux average
 300 lux average Electrical Outlet - Power supply for local heating, ventilation and air conditioning system.
 Electrical supply to hot water boiler and to any security and life safety systems necessary to facilitate Amazon's business.
 Electrical supply to any IT / Data equipment including distribution cabinets / IDP's.
 Five standard double socket outlets for general use and two standard double socket per workstation.
 Electrical feeding to sliding gates, fast opening gates and vehicle barriers.
 Data Port: Four RJ45 outlets per workstation
 Lighting Sensor: Motion sensor connected to lighting as per Design Criteria section 26-50-00

MECHANICAL INSTALLATION:

Winter Conditions:
 >21oC temperature
 >18oC temperature for secondary rooms
 <0.20m/s airspeed
 Summer Conditions:
 <24oC temperature
 <0.25m/s airspeed
 Relative Humidity: 00 - 70% Fresh Air:
 (WCs) : 36m³/h p.p or 14.4m³/h per m²
 50m³/h mechanical extraction Sensor: Temp. sensor connected to HVAC



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PROPOSED DEVELOPMENT LAND
 LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
 COVENTRY

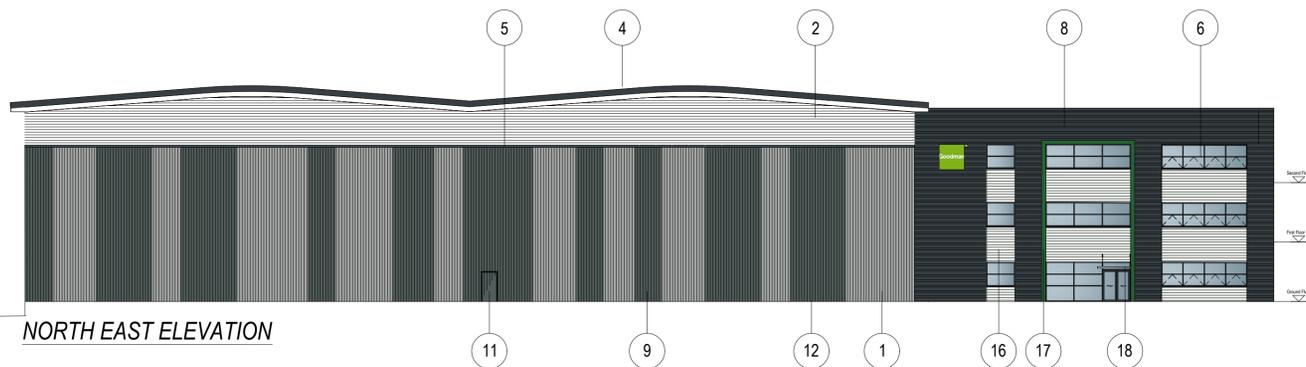
PRE-CHECK GATEHOUSE PLANS & ELEVATIONS

P3: Gatehouse updated following client comments 27/01/21
 P2: Drawing updated following client comments 14/10/20 RG
 P1: First Issue 05/10/20 JM
 Rev Date/Checked

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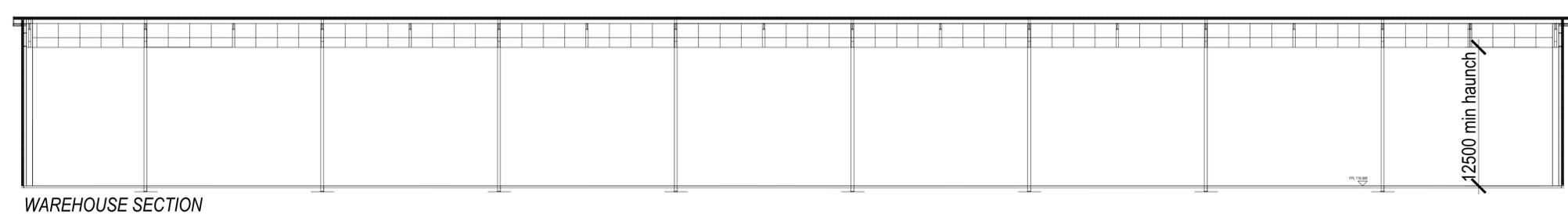
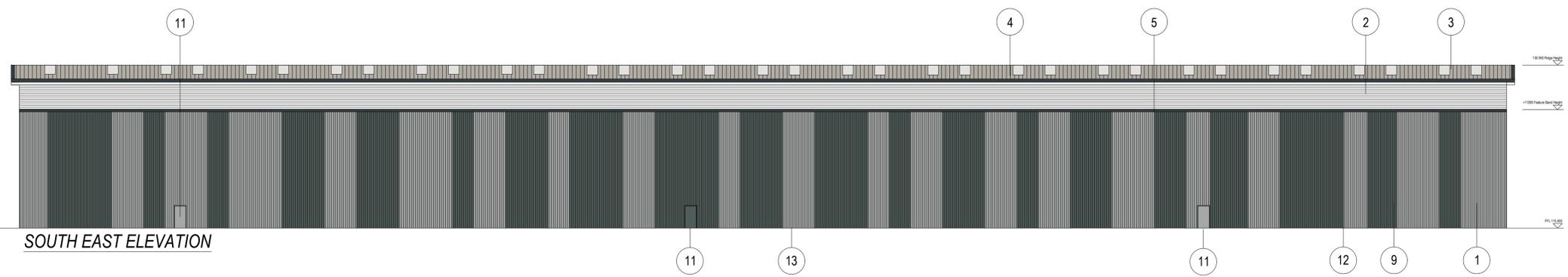
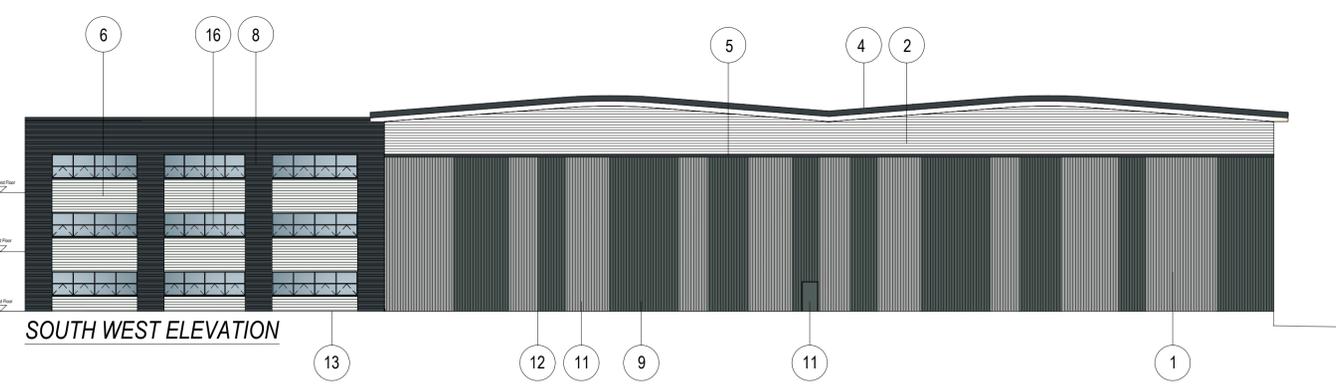
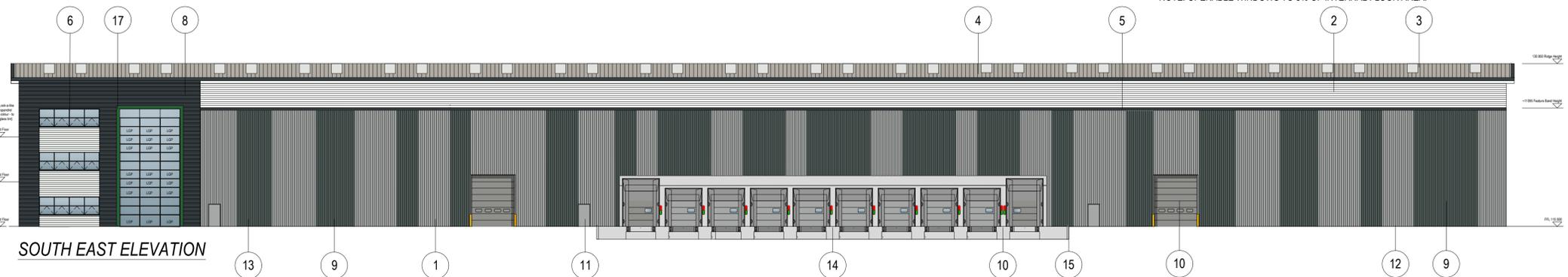
Issue Purpose:
 Drawn by: JM Checked by: KI
 Scale @ A1: 1:20 Date: 09/2020
 CAD ref: 3726 Dwg no.: 3726-1-203 Rev: P3

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- External Materials Key**
- Vertical Trapezoidal profile cladding in 'Silver Metallic' Light Grey RAL 9006
 - Horizontal Trapezoidal profile cladding in 'White' RAL 9003
 - Profile curved roofing sheets in HPS200 Ultra 'Goosewing Grey' 10A05
 - Two part eaves & verge flashing to upper part of flashing 'Anthracite' RAL 7016 and 'White' RAL 9003 to lower part
 - Feature band flashing in Anthracite RAL 7016
 - Windows polyester powder coated frames in Anthracite RAL 7016 with grey tinted anti-sun glass
 - Entrance door and curtain wall glazing frames in Anthracite RAL 7016 with grey tinted anti-sun glass
 - Horizontal CA 300MR Panel Profile cladding in Anthracite RAL 7016
 - Vertical Trapezoidal profile cladding in 'Slate Grey' Dark grey RAL 7012
 - Loading Dock and Level Access Doors Mid Grey RAL 9007
 - Steel fire exit doors and frame to match adjacent cladding Mid Grey RAL 9007 to match adjacent doors when located in concrete prowall
 - Galvanised steel channel plinth to warehouse
 - Bottom Drip flashing Anthracite RAL 7016 & White RAL 9003 to offices & 'Slate grey' Dark Grey RAL 7012 to warehouse
 - Precast concrete 'Prowall'
 - Concrete retaining walls with black dock levelers below dock access doors
 - Horizontal CA 300MR Panel Profile cladding in White RAL 9003
 - Feature band flashing surround to entrance / curtain walling in Goodman Green Pantone 369
 - Glass canopy over entrance supported off curtain wall frame

NOTE: OPENABLE WINDOWS TO 5% OF INTERNAL FLOOR AREA.



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PROPOSED DEVELOPMENT LAND
LYONS PARK, FORMER LAWRENCE AUTOMOTIVE SITE
COVENTRY

LYONS PARK, UNIT ELEVATIONS

Pl: Ridge height added.	10/11/20 rg
P5: FFL level added	28/10/20 rg
P4: Elevations updated to show curved roof cladding. Levels added	09/10/20 rg
P3: Additional Section added. Opening windows added	06/10/20 rg
P2: Updated following client comments	02/10/20 rg
P1: First issue	28/09/20 rg
Rev	Date/Checked
The Old Rectory Rectory Lane Milton Malsor NORTHAMPTON NN7 3AQ t: +44 (0)1604 858916 f: +44 (0)1604 859123 www.peter-haddon.com	php architects
Issue Purpose:	Planning
Drawn by: RG	Checked by: RM
Scale @ A1: 1/200	Date: 09/2020
CAD ref:	Dwg no.:
3726	3726-1-301
	P6
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- NOTES**
1. ELECTRICAL CONTRACTOR TO VERIFY LIGHTING DESIGN PRIOR TO INSTALLATION AS THIS IS ONLY A CONCEPTUAL DESIGN.
 2. ELECTRICAL CONTRACTOR TO READ THIS DRAWING IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS.
 3. ELECTRICAL CONTRACTOR TO ENSURE THE CAR PARK LUMINAIRES ARE INSTALLED TO COORDINATE WITH THE PARKING LOCATIONS.
 4. ELECTRICAL CONTRACTOR TO ENSURE CABLES AND LIGHTING COLUMNS ARE LOCATED SUCH THAT THEY WILL NOT BE AFFECTED BY THE FUTURE GROWTH OF THE TREE ROOT, ETC. IN THE AREA OF THE CAR PARK.
 5. ALL EXTERNAL LIGHTING IS TO BE CONTROLLED WITH A PHOTO CELL LOCATION TO BE CONFIRMED) SUCH THAT THE LIGHTING WILL BE ENERGISED AT LOW AMBIENT LIGHTING AND WILL REMAIN OPERATIONAL DURING THE HOURS OF DARKNESS.
 6. THE LIGHTING DESIGN TO COMPLY WITH THE CIBSE GUIDE 5008 THE OUTDOOR ENVIRONMENT. THE I.P. GUIDANCE NOTES FOR REDUCTION OF OBTRUSIVE LIGHT, 2020 AND ALL LATEST ADDITIONS.
 7. ROAD LIGHTING DESIGNED TO COMPLY WITH BS5489-1:2013 CODE OF PRACTICE FOR DESIGN OF ROAD LIGHTING.
 8. NO SHRUBBERY/VOLLAGES HAS BEEN ALLOWED WITHIN THE LIGHTING CALCULATION TO ALLOW FOR WORST CASE ON THE LIGHTING OVERSPLAT.

EX1 BUILDING MOUNTED HOLOPHANE D-SERIES LA364, FORWARD THROW OPTIC, 36000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 328 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED

EX2 COLUMN AND BUILDING MOUNTED HOLOPHANE D-SERIES LA164, FORWARD THROW OPTIC, 16000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 131 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED

EX1-BLS COLUMN MOUNTED HOLOPHANE D-SERIES LA364, FORWARD THROW OPTIC, 36000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 328 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED FITTED WITH BACKWARDS LIGHT SHIELD

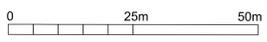
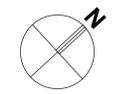
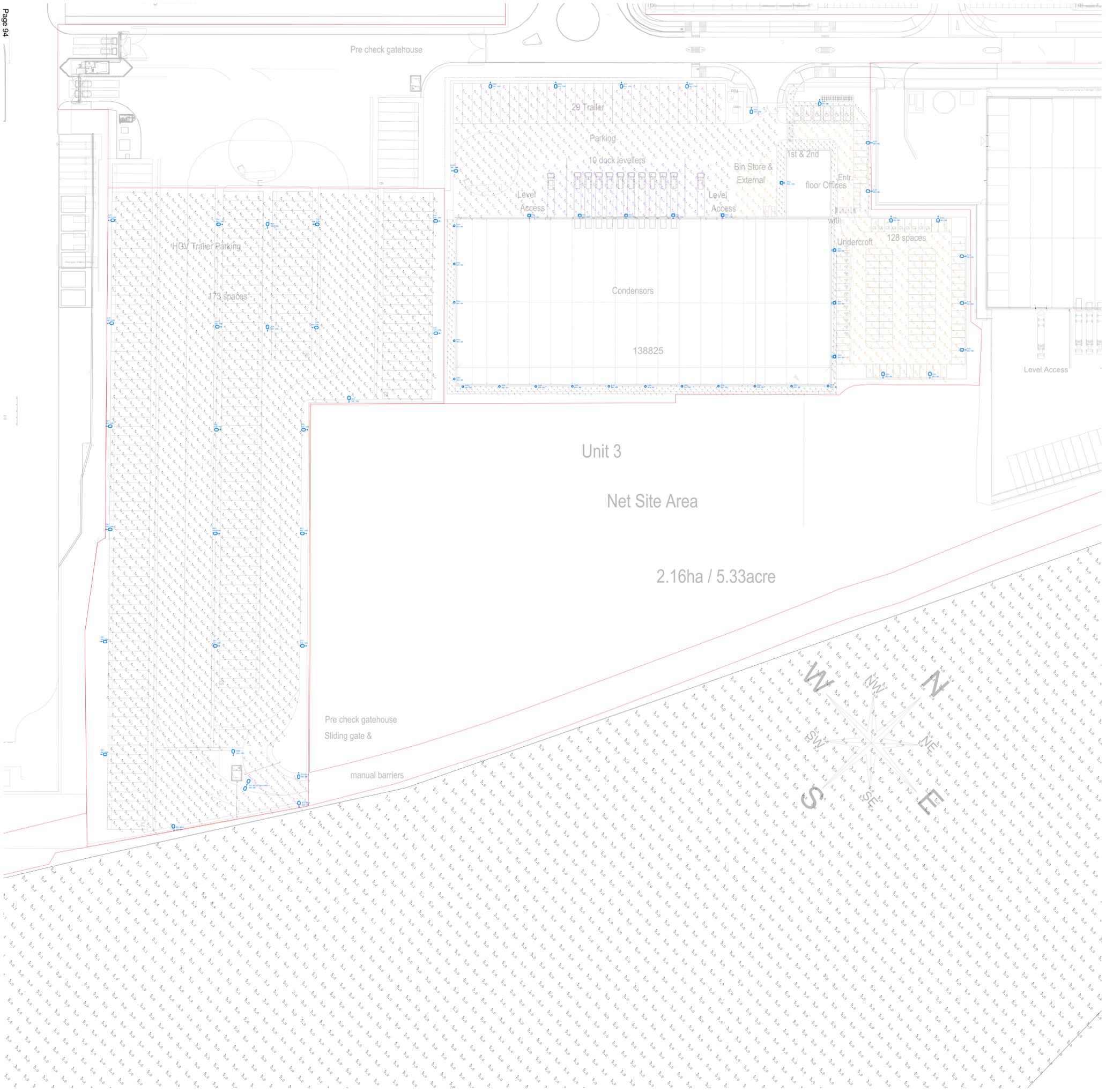
EX3 COLUMN AND BUILDING MOUNTED HOLOPHANE D-SERIES LA224, FORWARD THROW OPTIC, 22000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 208 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED

EX4 BUILDING MOUNTED HOLOPHANE DENVER ELITE LA014, NARROW OPTIC, PRISMATIC GLASS LENS, 1000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 12 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED

EX5 COLUMN AND BUILDING MOUNTED HOLOPHANE D-SERIES LA364, FORWARD THROW OPTIC, 36000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 328 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED

EX6 COLUMN MOUNTED HOLOPHANE D-SERIES LA364, SYMMETRIC OPTIC, 36000 INITIAL LUMENS PER LAMP COLOUR TEMPERATURE: 4000K MAINTENANCE FACTOR = 1 WATTS PER LUMINAIRE = 328 TILT ANGLE= 0° MOUNTING HEIGHT= AS DETAILED

AREA	AVG LUX	MAX LUX	MIN LUX	UNIFORMITY
BUILDING PEDESTRIAN	20.61	41.20	9.80	0.48
BUILDING PERIMETER	13.04	37.40	2.90	0.22
CAR PARK PEDESTRIAN	17.10	27.60	4.20	0.25
CAR PARKING	17.93	28.00	8.50	0.47
DOCK LEVELLERS	53.83	80.00	33.90	0.62
HGV PARKING	31.99	78.20	14.00	0.44
PRE CHECK OFFICE	100.00	138.60	60.40	0.61
SERVICE YARD	30.77	55.20	17.30	0.56
SERVICE YARD CAR PARK	33.39	44.30	24.70	0.74



THIS DRAWING IS AN INDICATIVE LAYOUT ONLY, DEMONSTRATING THE CONCEPTUAL REQUIREMENTS CORRELATING WITH THE AGREED SCHEMATIC SOLUTIONS. THE PURPOSE OF THIS DRAWING IS TO DEMONSTRATE DESIGN INTENT. FOR FURTHER DEVELOPMENT BY THE RELEVANT SERVICES DESIGNER ONLY.

THIS DRAWING IS TO BE READ IN CONJUNCTION WITH THE ELECTRICAL PERFORMANCE SPECIFICATION & ALL RELEVANT ARCHITECTURAL & ENGINEERING DRAWINGS. DETAILED & FULLY COORDINATED INSTALLATION DRAWINGS SHALL BE PROVIDED BY THE ELECTRICAL CONTRACTOR IN ACCORDANCE WITH THE SPECIFICATION.

PI	2019/02/01	LAMP FIXTURES AND LAYOUT TO REFLECT LATEST BEST PRACTICE	VS	MS
PI	2019/02/05	REQUIRED TO REFLECT DESIGN TEAM COMMENTS	MS	MS
PI	2019/03/05	INFORMATION FOR COMMENT	MS	MS
REV	DATE	DESCRIPTION	BY	VC

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RIBA STAGE 2 RIBA STAGE 3 RIBA STAGE 4

INFORMATION ONLY

CLIENT
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PROJECT
PROPOSED DEVELOPMENT LAND
LYONS PARK FORMER LAWRENCE
AUTOMOTIVE SITE, COVENTRY

DRAWING TITLE
PROPOSED EXTERNAL LIGHTING LAYOUT

SCALE (M): 1:500 DATE: OCTOBER 2020 BY: VS

ISSUING NUMBER: CPW-200778-E-200-EXT-0001 P3

Planning Committee Report	
Planning Ref:	FUL/2021/0315
Site:	97 Broad Lane
Ward:	Earlsdon
Proposal:	Change of use from (C3) Dwellinghouse into (C2) Children's Care Home for four children
Case Officer:	Darren Henry

SUMMARY

The application seeks to change the use of the property to provide a care home for vulnerable children between the ages of 6 – 17 who have social and emotional difficulties. The number of children in the care home will be limited to five and they will be supported by three members of staff at all times on a rota basis, including at night. The Planning Statement states that all of the children in care will attend mainstream schools or colleges during normal school term times. At all other times, the children will be resident within the home as a single household, not too dissimilar to a family, engaged with structured activities.

BACKGROUND

The application site is located within a suburban area of Coventry, within the ward of Earlsdon. The surrounding area is predominantly housing.

Although the Council has no planning record, the property was previously registered with Ofsted for use as a home for children with Autism and learning difficulties, quite different from what is being proposed here, which is a care home for children with social and emotional behavioural difficulties, whereby routines will be in place to create a more balanced and structured pattern of living not too dissimilar to how families go about their lives.

A children's care home comes under a C2 use class (Residential Institutions). Other uses under this use class include other care homes, hospitals, nursing homes, boarding schools, residential colleges and training centres. It should, however, be noted this class is separate from a C2a (Secure Residential Institution) use class which covers young offender's institutions, prisons, secure hospitals etc.

KEY FACTS

Reason for report to committee:	16 letters of objection have been received as well as a petition with 33 signatories objecting to the proposal.
Current use of site:	Existing residential property
Proposed use of site:	To change the use to a care home for children.

RECOMMENDATION

Planning committee are recommended to grant planning permission subject to the conditions listed within the report.

REASON FOR DECISION

- The proposal is acceptable in principle.
- The proposal will not adversely impact upon highway safety.
- The proposal will not adversely impact upon the amenity of neighbours.
- Together with the aims of the NPPF, the proposal accords with Coventry Local Plan Policies:

AC2: Road Network

AC3: Demand Management

DE1: Ensuring High Quality Design

DS3: Sustainable Development Policy

H8: Care Homes, Supported Housing, Nursing Homes and Older Persons accommodation

APPLICATION PROPOSAL

The application seeks to change the use of the building from a dwelling (C3) to a children's care home (C2), thereby remaining as a residential property. The property will provide a safe environment for four vulnerable children between the ages of 6 – 17, along with two live-in members of staff, plus a further professional member of staff to provide 24-hour on-site support, in strict compliance with OFSTED'S required operating guidelines.

The proposed use will result in the loss of a single dwelling house; however, the loss of one housing unit is considered not to have a significant detrimental impact on the Council's overall housing delivery strategy.

SITE DESCRIPTION

The application site is located on the south side of Broad Lane, with No. 91 to the east and No.99 to the west. The property is detached, with gable ends to the front and rear and the main entrance located to the side facing No.91.

To the front is a good-sized garden which has been hard-landscaped, with a driveway leading to a garage to the rear. The side boundaries of the front garden consist of mature hedging and continue to the rear, where there is an extensive rear garden, bounded by mature hedging with No.99 and panel fencing with No.91.

PLANNING HISTORY

There have been a number of historic planning applications on this site, as set out below:

Application Number	Description of Development	Decision and Date
R/1999/0889	Construction of replacement dormer detached bungalow and detached garage	Approved 2 nd December 1999
R/2000/0986	Construction of replacement dormer detached bungalow and detached garage	Approved 23 rd May 2000

	(Discharge of Condition No. 2 (in part) samples of external facing and roofing materials)	
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POLICY

National Policy Guidance

National Planning Policy Framework (NPPF). The NPPF sets out the Government’s planning policies for England and how these are expected to be applied. It sets out the Government’s requirements for the planning system only to the extent that is relevant, proportionate and necessary to do so. The NPPF increases the focus on achieving high quality design and states that it is “fundamental to what the planning and development process should achieve”.

The National Planning Practice Guidance (NPPG) adds further context to the NPPF and it is intended that the two documents are read together.

Local Policy Guidance

The current local policy is provided within the Coventry Local Plan 2016, which was adopted by Coventry City Council on 6th December 2017. Relevant policies relating to this application are:

AC2: Road Network

AC3: Demand Management

DE1: Ensuring High Quality Design

DS3: Sustainable Development Policy

H8: Care Homes, Supported Housing, Nursing Homes and Older Persons accommodation

Supplementary Planning Guidance/ Documents (SPG/ SPD):

SPD Coventry Connected

CONSULTATION

No objections subject to conditions received from:

- Coventry City Council Environmental Protection
- Coventry City Council Highways
- West Midlands Police

16 letters of objection have been received, along with a petition with 33 signatories objecting to the proposal, stating the following material planning considerations:

- When Rowan House Care Home was active there was Anti-social behaviour problems on a regular basis, missing persons, drug taking and criminal damage.
- It is an antiquated myth that putting people with problems in an affluent area is a good thing. It is misleading.
- Around the corner from the care home is one of the poorest housing estates in Coventry, which has a large level of minor crime and will be attractive to children with issues.

- Approval of this application will mean houses at 97, 135, and 162 Broad Lane, which are located within around 200m of each other, have the same use. These facilities should be spread across appropriate districts rather than concentrated in specific areas.
- The use will create exponential noise.
- Concerned for personal safety.
- Changing the status for the subject property from a dwellinghouse to a childcare home is not in keeping with properties within the immediate vicinity.
- The inclusion of three permanent members of staff on site as highlighted will inevitably see an increase in movement, noise and potential disturbance.
- The age band of 6 to 17 is very wide. However, the children are referred to as young people in the planning statement and not children which indicates all residents will be teenagers.
- The increase in this footfall will have a significant increase on noise levels which will be even more unacceptable to what we have previously experienced. The increase in footfall will also impact on our privacy due to proximity to neighbouring property.
- In section 5.11 of the planning statement it states that children living in care homes that are located in prosperous areas, such as Broad Lane, gain the confidence and aspirations to seek and secure highly skilled and long-term employment. Where is the evidence for this? A similar care home for 11 – 16-year olds was established on Broad Lane. The home was closed down after 19 months due to complaints from local residents due to the negative impact on the economic, social and environmental conditions of the area. This clearly demonstrates this type of dwelling is not beneficial.
- Object to the fact that it is close to a staggered junction with traffic lights. We have lived in Broad Lane for 11 years and there has been several accidents at this junction.
- Several comments received in relation to not receiving a consultation letter.
- If the residents are youth offenders, or being housed until they are old enough to be placed in secure institutions, there would be potential for anti-social behaviour e.g. hiding in people's gardens, damage to property, burglaries, groups of their friends congregating at entrance to property, or on the pavements nearby, plus police cars attending on a regular basis.
- The parking is very inadequate for use as a children's home due to the narrow driveway at the side of the property and only one parking space at the front of the property. When staff change over, it will be necessary to shuffle vehicles in and out of the driveway on to Broad Lane which disrupts traffic at the traffic light-controlled box junction with Wildcroft Road and Broadlands Close.
- The impact of children living at the address especially those with behavioural problems will have an impact on the elderly and other residents re noise pollution and possible anti-social behaviour problems as per the previous care home on broad lane a few years ago.

One letter of support has also been received, stating:

- There is great need for care homes for vulnerable children, particularly ones in a nurturing environment where there are opportunities to gain independence and prepare young people for the real world.

The Agent has also provided a number of benefits likely to result from the proposal, including:

- The care home will employ highly skilled, experienced staff to ensure the children have every opportunity to grow and thrive in a safe and nurturing environment.
- The children will experience a positive family lifestyle, in high-quality accommodation for, in some cases, the first time in their lives.
- The children will benefit from invaluable advice, guidance and direction
- The care home will ensure full attendance by each of the five children at the local schools, and that the children receive wholesome meals and that dental and doctor's appointments are fulfilled.
- The children will also receive access to broad leisure and recreational facilities, and a commitment to building upon each of their strengths.
- The proposal, which requires a number of support staff, will bring employment to Coventry, boosting its Health & Social Care sector.
- As a result of the support the children receive, they will receive have the best chance possible of obtaining employment, bolstering Coventry's economy in the process.
- The children will be encouraged to engage in fundraising events and other community activities to give them a sense of belonging,
- The proposal will support Local authorities by helping them to accommodate children at risk.

APPRAISAL

The main issues in determining this application are: principle of development, the impact upon neighbouring amenity, design and visual appearance, landscape and trees, impact on highway safety and heritage

Principle of development

The site is in a residential area within a sustainable location close to the City Centre where there are excellent facilities and services within easy reach by foot and buses. The area is close to parks and open spaces to spend quality time and self-reflect, conducive to healthy living. Furthermore, the change of use would be supported by Policy H8, which states that *"(1) Proposals for care homes, nursing homes and other specialist and supported forms of housing for the elderly and those requiring care will be encouraged in areas that are accessible by a choice of means of transport and that are situated in close proximity to key local services, and (2) Proposals should be of a high quality and design and be compatible with the character of the surrounding area"*.

The proposal is in accordance with Policy H8 as the site is within a sustainable location, close to schools, job opportunities and sustainable transport modes and its use will not detract from the existing character and appearance of the area.

It is therefore considered that the principle of the development is acceptable, unless material matters prove otherwise.

Impact on residential amenity

The way buildings relate to each other must provide and protect acceptable levels of amenity for both existing and future residents. Therefore, the orientation and separation distance must be holistically thought out, taking into account of front, rear and side facing windows to habitable rooms, as these will be protected from significant overlooking and overshadowing where such windows are the primary source of light.

The applicant states that the care home will be run similar to a family home. Indeed, the building itself was constructed as a dwelling and would generally retain a standard arrangement of rooms found in most homes, albeit with a staff room, including 7 bedrooms, 4 for the children and 2 for the staff, a lounge, kitchen and 2 bathrooms. In terms of the outside amenity space there is a large rear garden and patio area.

Impact on privacy

There would be no changes to the property in terms of the position and size of windows in the existing dwelling. Objectors have however raised issues with regard overlooking of properties to the front. It is however considered there would be no difference to the potential of overlooking from that of the existing use of the building.

Therefore, there would not be a significantly greater impact from overlooking and loss of privacy as a result of the proposed use as compared to that of the existing dwelling.

Noise and disturbance

Objectors have raised concern over noise and disturbance, stating the area is a quiet residential area and that the proposal will result in excessive noise, as well as light pollution disturbance from shift changes and the comings and goings of traffic.

In response to these issues, the property is relatively large, detached and is set within its own grounds with plenty of off-street parking. The number of staff required is low and the scale of the running of the care home would more than likely not require significant comings and goings, no more unusual than a normal family. There will always be two trained staff on-site. The two live-in staff will operate on a shift pattern of two days on, four days off and handovers will be around 10am or 11am. The carers will have their own bedrooms to sleep in every night, and the carers will alternate every 2 days.

This means that there will be care staff in the house 24 hours a day, seven days a week, who will be supported by a Manager with Level 5 Leadership from Mondays to Fridays between 9:00am to 5:00pm.

During unsociable hours, like most homes, it is expected the residents would be more than likely asleep or within the house. Therefore, whilst there may be some increased impact; it is considered likely that there would not be a significantly greater impact from noise and disturbance than could be experienced from the existing use.

Coventry City Council Environmental Protection Officers were consulted and did not raise any issues with regards to noise and disturbance.

Impact on the character of the area

Section 12 of the National Planning Policy Framework 2019 (NPPF) outlines the Government's commitment to good design and attaches great importance to the design of the built environment, highlighting it as a key aspect of sustainable development. Consequently, decisions should aim to ensure that developments are visually attractive as a result of good architecture and appropriate landscaping.

The NPPF further states that "where the design of a development accords with clear expectations in plan policies, design should not be used by the decision-maker as a valid reason to object to development". However, "Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions, taking into account any local design standards or style guides in plans or supplementary planning documents" (130).

Furthermore, Policy DE1: Ensuring High Quality Design, states: "All development proposals must respect and enhance their surroundings and positively contribute towards the local identity and character of an area". In reference to new housing Policy H3 states that new residential development must provide a high-quality residential environment to help urban regeneration and create sustainable communities with adequate amenity space and car parking provision and be safe from environmental pollutants, excessive noise and air quality issues.

In terms of the character of the area, objectors have stated that the area's character would change for the worse due to the proposal and the commercial aspect of the proposed care home. Whilst there may be a slightly more intensive use of the property, in all probability, due to the small scale of the proposal and it being run in a similar way to that of a family household, it is considered that proposed use will not be any different from that of its current use as a C3 dwelling.

It is therefore considered the proposal would not warrant a refusal reason on this issue.

Perception and Fear of Crime and Anti-Social Behaviour

Objection has been received from local residents who suggest this proposal would result in an increase in crime, vandalism, anti-social behaviour and police call-outs in the area and that this would affect the amenity of the residents and character of the area, referencing issues at previous care homes in the vicinity.

Whilst the objector's concerns are noted and highlight potential issues that can result from the running of care homes, there is no actual link between this care home and others previously in the area. It could be the case that the proposed children's home would not result in such significant issues. The behaviour of the occupiers, however, would be a matter for the individuals in the home and the management of it.

In one case law example (Bromley 02/12/1994 DCS No 033-844-797) a Planning Inspector states "that the frequency of bad behaviour would be difficult to predict and would depend on individual children and the supervision they received. These were personal matters not concerned with the use of the property".

Whilst in the opinion of objectors, the home would result in crime and anti-social behaviour to the detriment of the character of the area and the community, it should be tempered with the fact that the home will be staffed 24/7 by care workers and a manager and that the home will be registered with OFSTED.

Whilst a children's care home does not require internal CCTV cameras, nor would that be Ofsted compliant, the applicants would be willing to accept CCTV on the outside of the property to safeguard children from any potential intruders. The use of external CCTV would be solely for the purpose of security. Furthermore, the WM Police have recommended several Secured By Design Measures to help reduce the potential for crime at the property, which if approved, will be imposed as a condition of the approval.

Whilst any incidences of anti-social behaviour would not be welcomed, there is no evidence that such behaviour will occur at the proposed children's care home. Indeed, it is much more likely that the children will benefit from a structured lifestyle akin to how a family operates its daily routines, as opposed to entering a life of crime or causing anti-social behaviour. Therefore, it is considered that the use of the land, as a care home, would not warrant a refusal for this issue.

Parking and Highway Safety

The property benefits from a garage and a large driveway for four cars, three of which will be for staff and one available for a visitor. There are also good links to public transport for sustainable travel options.

Objectors have detailed that the off-street parking may not be usable as they state the access is dangerous and is next to a junction with traffic lights. If permission is granted it is unlikely that on-street parking would occur as it is restricted due to double yellow lines.

Coventry City Council Highways were consulted and have stated that in respect of this application the Highway Authority considers that the impacts of the development are not severe and has no objections, subject to conditions relating to bicycle storage.

Other Issues

Comments have been made regarding the legalities of the consultation and stating that wider consultation should have taken place. The Development Management Procedure Order sets out the minimum requirements for consultation which, in this case, would require adjoining neighbours or a site notice. The consultation carried out has been far more extensive than required.

Equality Implications

Section 149 of the Equality Act 2010 created the public sector equality duty. Section 149 states:-

(1) A public authority must, in the exercise of its functions, have due regard to the need to:

- a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;

- b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

Officers have taken this into account and given due regard to this statutory duty, and the matters specified in Section 149 of the Equality Act 2010 in the determination of this application.

There are no known equality implications arising directly from this development.

Conclusion

The proposed development is considered to be acceptable in principle and will not result in any significant impact upon neighbour amenity, character and visual appearance of the area and any highway issues and therefore recommended to Members grant planning permission subject to relevant conditions. Together with the aims of the NPPF, the reason for Coventry City Council granting planning permission is because the development is in accordance with the following Coventry Local Plan 2016 Policies: AC1, AC3, AC4, DE1, and H8.

CONDITIONS:/REASONS

1. The development hereby permitted shall begin not later than 3 years from the date of this decision.

Reason: *To conform with Section 91 of the Town and Country Planning Act 1990 (as amended).*

2. The development hereby permitted shall be carried out in accordance with the following approved plans: Location Plan P20-1398-01-001; Existing Block Plan P20-1398-02-001; Existing Floor Plans P20-1398-03-001; Proposed Block Plan P20-1398-01-004-001a; Proposed Floor Plans 139 GA001A Issue 2.

Reason: *For the avoidance of doubt and in the interests of proper planning.*

3. Prior to occupation of the Care Home hereby permitted, details of cycle parking facilities shall be submitted to and approved in writing by the Local Planning Authority. The cycle parking facilities shall be provided in full accordance with the approved details prior to first occupation of the building and thereafter those facilities shall remain available for use at all times and shall not be removed or altered in any way.

Reason: *In the interests of encouraging the use of alternative modes of transport with the aim of creating a more sustainable city in accordance with Policies DS3, AC3 and AC4 of the Coventry Local 2016.*

4. Any gas boilers installed on site shall have a dry NOx emission rate of no more than 40mg/kWh. One electric vehicle recharging point per dwelling shall be

provided prior to occupation and shall not be removed or altered in any way and shall be kept available at all times

Reason: *To mitigate the impacts of development on air quality in accordance with Policy DS3 of the Coventry Local Plan 2016 and the aims and the objectives of the NPPF (RAQ2).*

5. Prior to occupation of the development hereby permitted, an on-site Management Plan to include Secured by Design security measure shall be submitted to and approved in writing by the local planning authority. Details to be submitted shall include:
- i. First floor windows with limiters
 - ii. Installation of an Intruder alarm
 - iii. Installation of external CCTV cameras at the front and back of house
 - iv. Retention of 1.8 metre boundary fence to the sides and rear of property.

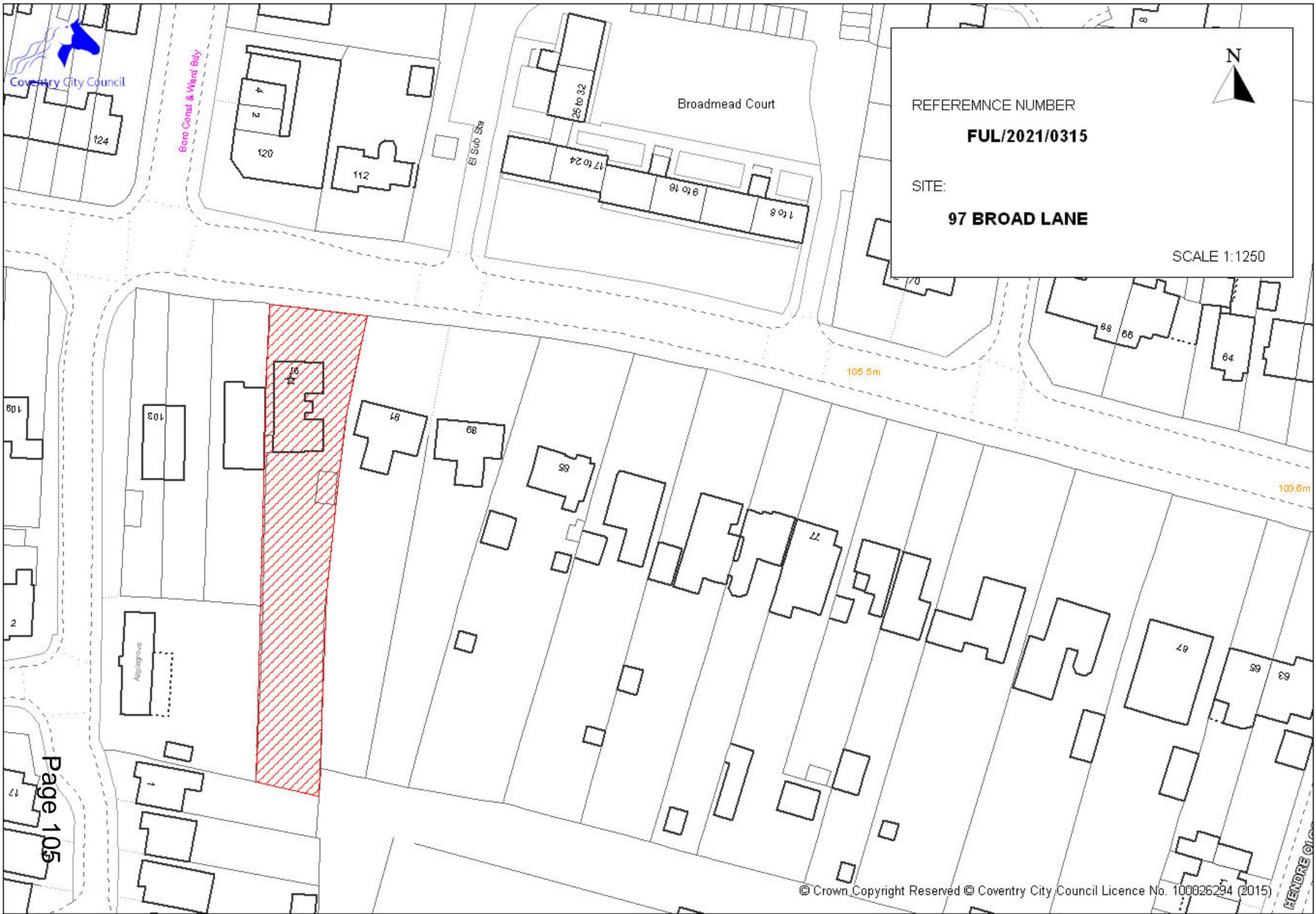
Reason: *To ensure Secured by Design standards are met, in the interests of safety and security and the health and wellbeing of future occupiers of the development in accordance with Policies DS3, H3 and DE1 of the Coventry Local Plan 2016.*

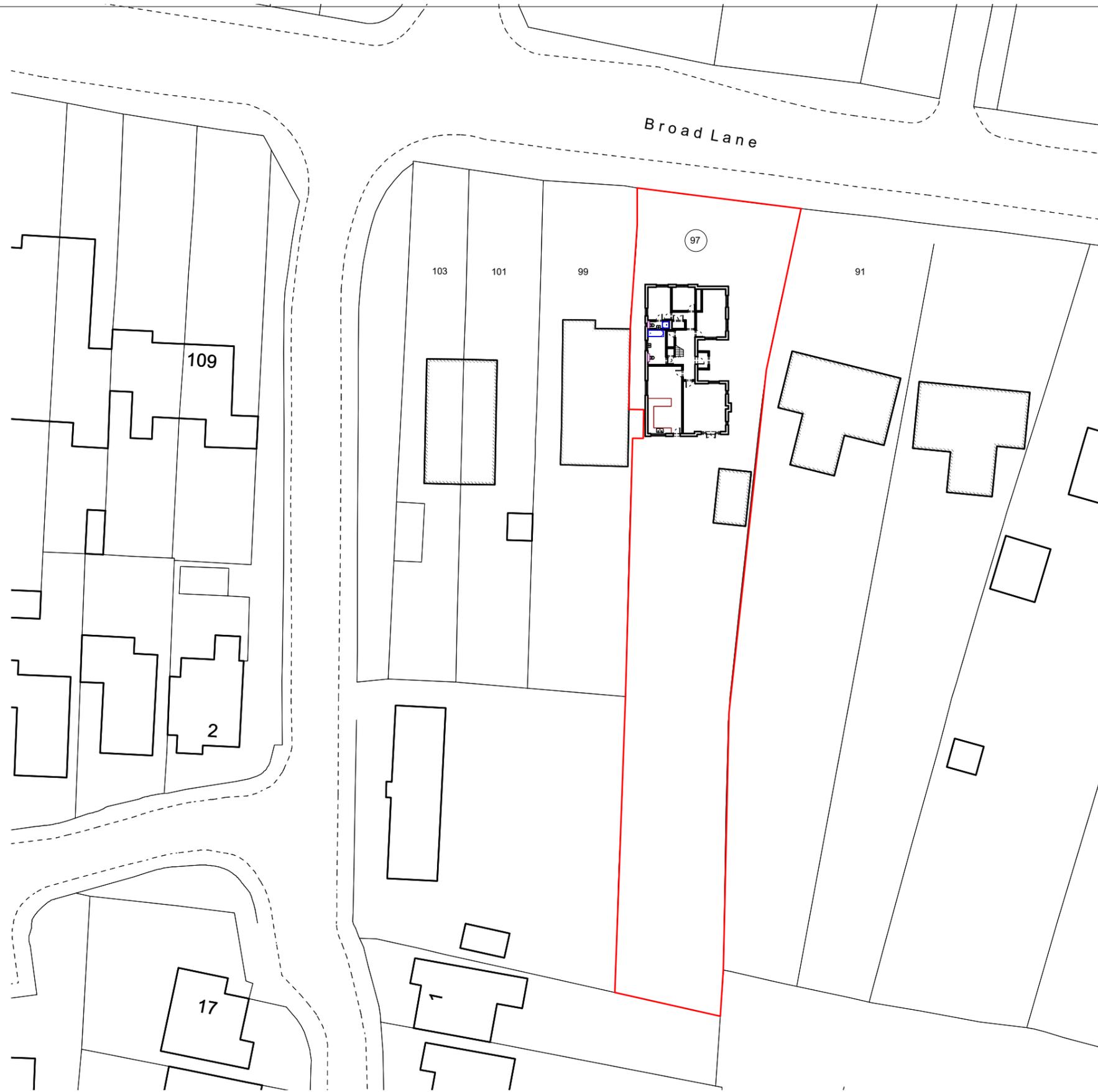
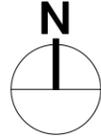
Boro Conest & Ward Boly

REFEREMNCE NUMBER
FUL/2021/0315

SITE:
97 BROAD LANE

SCALE 1:1250





1 Existing - Block Plan
Scale: 1:500



General Notes

A. GENERAL NOTES:

All dimensions to be checked on site by the Contractor prior to construction and the Architect to be informed of any discrepancies. All dimensions are in millimetres unless otherwise stated. This drawing to be read in conjunction with all relevant Engineers and other specialist drawings. All work to be carried out to the approval of the Building Inspector. Contractor to provide full setting out drawings for approval by architect prior construction.

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2. While this drawing can be used as a base drawing for construction purposes, it is VERY IMPORTANT that all dimensions are checked carefully before any work commences or any materials are ordered.

3. This drawing can be used as part of a planning application, although the planning officer may ask for more specific information about some aspects of the design.

4. Where applicable, a suitable Structural Engineer and/or a Party Wall Surveyor should be consulted. Although as far as possible these instances have been indicated, this is not necessarily exhaustive and the whole scope of proposed works should be reviewed.

5. Unless other arrangements have been specifically made, the building contractor should serve a Building Notice, as and where applicable, to the local authority to satisfy the requirements of the Building Regulations. The building contractor should also liaise with the Building Control Officer regarding routine inspections of the work.

The drawing intended to illustrate the general arrangement of the project proposals. As it stands this drawing does not include all of the detail necessary for a full plans building regulations application.

While this drawing can be used as a base drawing for construction purposes, the building contractor will require more information. It is therefore important to discuss, with NKA & the builder together, where more detail would be appropriate.

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Planning

Issued for Planning	04/02/2021	1
Issue Notes	Date	No.

Project ID

Dimensions Care

Sheet Title
Existing Block Plan

Project Title
97 Broad Lane, Coventry, CV5 7AH

CAD File Name

Scale	Date
As Shown	04.02.21

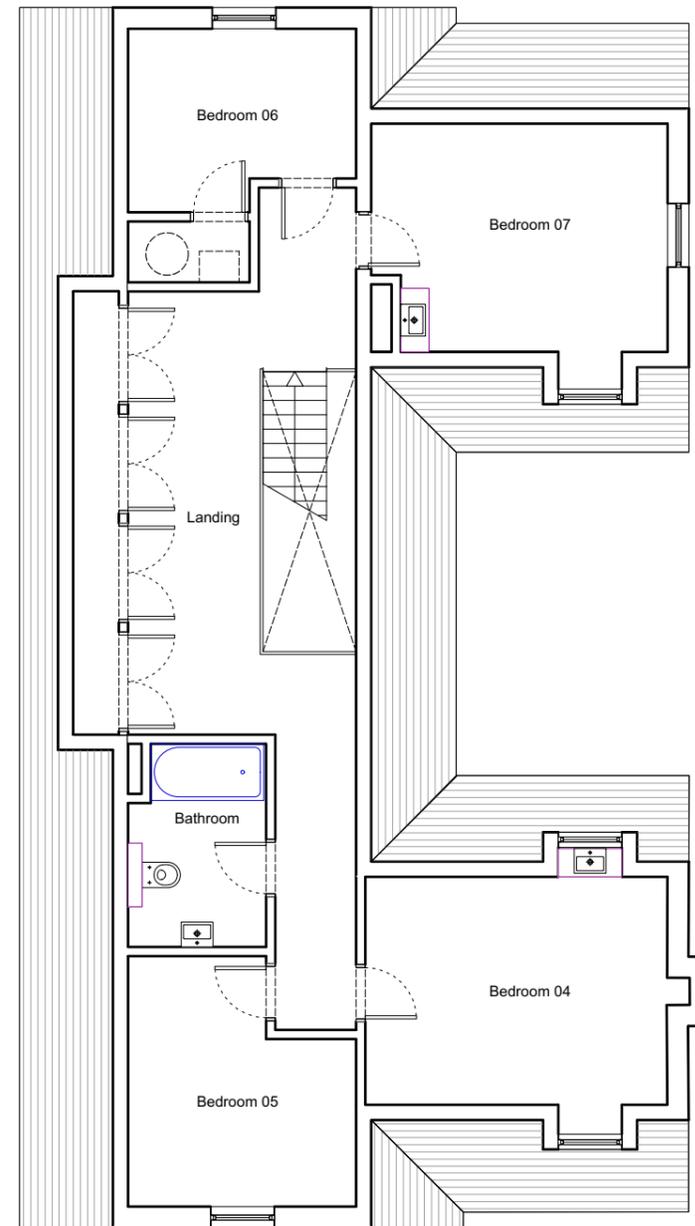
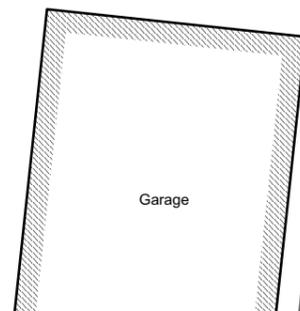
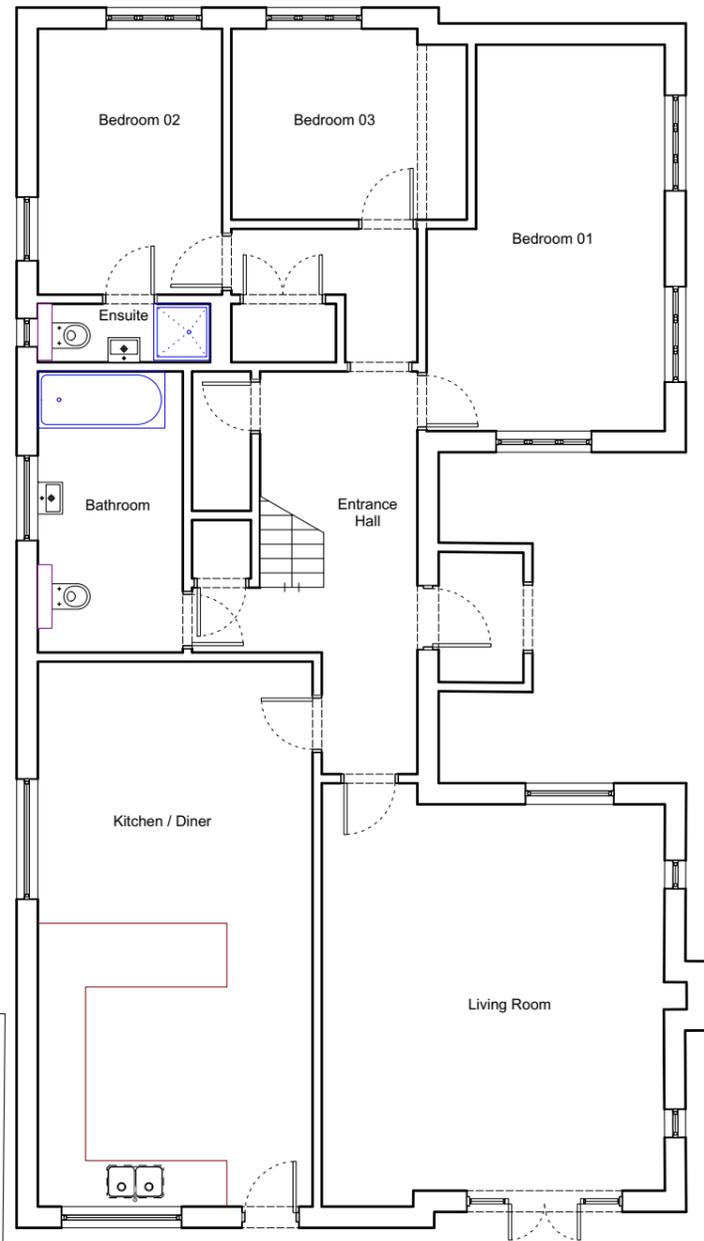
Checked By	Sheet No.
NKA	139 EX001

Drawn By
NK

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2 Proposed - First Floor Plan
Scale: 1:100



General Notes

A. GENERAL NOTES:

All dimensions to be checked on site by the Contractor prior to construction and the Architect to be informed of any discrepancies. All dimensions are in millimetres unless otherwise stated. This drawing to be read in conjunction with all relevant Engineers and other specialist drawings. All work to be carried out to the approval of the Building Inspector. Contractor to provide full setting out drawings for approval by architect prior construction.

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3. This drawing can be used as part of a planning application, although the planning officer may ask for more specific information about some aspects of the design.

4. Where applicable, a suitable Structural Engineer and/or a Party Wall Surveyor should be consulted. Although as far as possible these instances have been indicated, this is not necessarily exhaustive and the whole scope of proposed works should be reviewed.

5. Unless other arrangements have been specifically made, the building contractor should serve a Building Notice, as and where applicable, to the local authority to satisfy the requirements of the Building Regulations. The building contractor should also liaise with the Building Control Officer regarding routine inspections of the work.

The drawing intended to illustrate the general arrangement of the project proposals. As it stands this drawing does not include all of the detail necessary for a full plans building regulations application.

While this drawing can be used as a base drawing for construction purposes, the building contractor will require more information. It is therefore important to discuss, with NKA & the builder together, where more detail would be appropriate.

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Planning

Issued for Planning	04/02/2021	1
Issue Notes	Date	No.

Project ID

Dimensions Care

Sheet Title
Proposed Floor Plans

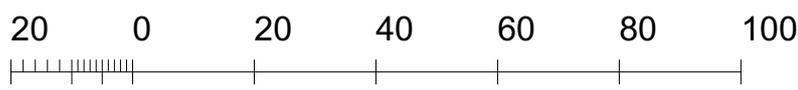
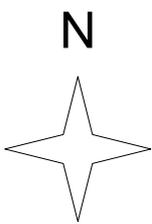
Project Title
97 Broad Lane, Coventry, CV5 7AH

CAD File Name

Scale	Date
As Shown	04.02.21

Checked By	Sheet No.
NKA	139 GA001
Drawn By	
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Scale = 1:1250 @ A4

Metres

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Planning Committee Report	
Planning Ref:	FUL/2021/0369
Site:	Land at Westwood Heath Road, to the rear of St John's Church
Ward:	Westwood
Proposal:	Change of use of land from agricultural use to public garden.
Case Officer:	Darren Henry

SUMMARY

The application seeks to change its current use from a paddock land to an informal outdoor public recreation space associated with and administered by the Church, as well as being open to the general public.

BACKGROUND

The proposals are for the site to be used as a garden during daylight hours, to be used by the community and managed and administered by St John's Church. The principal access into the site is via a field gate off Featherbed Lane.

KEY FACTS

Reason for report to committee:	The application has received 1 letter of objection and 40 letters of support.
Current use of site:	Agricultural (paddock)
Proposed use of site:	To change the use to a public garden

RECOMMENDATION

Planning committee is recommended to refuse granting planning permission.

REASON FOR DECISION

a) The proposal is not acceptable in principle.

APPLICATION PROPOSAL

The proposals are for the site to be used as a garden by the community, managed and administered by St John's Church. The proposal will also include a car parking area, space for a mobile café van and erection of a temporary Garden Room structure, along with soft and hard landscaping.

SITE DESCRIPTION

The application site is located on the eastern side of Featherbed Lane. To the north is a Vicarage and beyond this to the northeast is St John's Church, to which the field belongs. To the west of the site is No.2 Featherbed Lane and to the south are the properties of Meadow View and Clear View. Southeast of these are Warwick University halls of residence.

The applicant's Design and Access Statement states that the field was last used as an agricultural field in 2017 when it was stocked with horses. Since then the majority of the site has been fallow, left as uncut grass, until it was cut at the end of 2020. An exception to this is an area measuring approximately 36 metres x 15 metres that has been laid with reinforced grass and employed by the church as overflow car parking when there have been special services at the church, such as weddings or at Christmas/Eastertime.

The application site is bounded by hedgerows along all sides of the site, but they vary in height from as low as 1.5 metres to 7 metres.

The site is safeguarded land between the urban area and the Green Belt which is identified to meet longer-term development needs beyond the plan period.

PLANNING HISTORY

No relevant recorded history.

POLICY

National Policy Guidance

National Planning Policy Framework (NPPF). The NPPF sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that is relevant, proportionate and necessary to do so. The NPPF increases the focus on achieving high quality design and states that it is "fundamental to what the planning and development process should achieve".

The National Planning Practice Guidance (NPPG) adds further context to the NPPF and it is intended that the two documents are read together.

Local Policy Guidance

The current local policy is provided within the Coventry Local Plan 2016, which was adopted by Coventry City Council on 6th December 2017. Relevant policies relating to this application are:

AC1: Accessible Transport Network

AC3: Demand Management

AC4: Cycling and Walking

DE1: Ensuring High Quality Design

DS1: Overall Development Needs

DS2: The Duty to Cooperate

GB2: Safeguarded Land in the Green Belt

CONSULTATION

Objections received from:

a) Coventry City Council Planning Policy

No objections received from:

b) Coventry City Council Highways

No objections, subject to conditions, from:
c) Coventry City Council Urban Design and Landscape

Neighbour notification was sent in accordance with the Communications Record and a site notice was posted on the 4th of March 2021.

There has been 1 letter of objection raising the following material planning consideration:

- Object to the proposal due to concerns about access to the proposed public garden. The lane is a single access track to which currently there can be issues to access the lane particularly on entering or exiting the lane. If it was widened the objection would be removed.

There have also been 40 letters of support for the following reasons:

- Currently it is waste ground and if converted into a garden it will enhance the area by giving people the opportunity to relax in a tranquil countryside environment.
- The plan to create a garden where people can either come and be alone or be welcomed into the community of those who will be caring for the garden seems is what is needed. It will be a space for outdoor recreation, created to be in sympathy with local flora and fauna; nature will be allowed to flourish, and the use of land in this way seems well within its green belt status. Those with disabilities might find it difficult to trek out into the fields and go for a walk but could have the opportunity of enjoying time in a garden with wheelchair access.
- This is a temporary project to provide a quiet reflective space where people who have been locked down for months and walk, talk and improve their mental and spiritual health. So, I see no reason why it being Green Belt should be a problem.
- This garden will provide significant benefit to the community as well as caring for the land that it is on. Providing recreational activities on green belt land should be encouraged, especially as this will help people better connect and care for the environment around them. The garden would improve the quality of the land, increase biodiversity and promote communal well-being.
- This garden will not harm the green belt area after all someone granted permission for the high-speed train, which has torn up the countryside having terrible effects on nature.
- The garden is a 2-year project and will revert to a field at the end of the period thus the green belt will be unaffected in the medium term.

APPRAISAL

The main issues relating to this application are the principle of the development, the design and appearance and the impact on neighbouring amenities.

Principle of development

The Duty to Cooperate was created in the Localism Act 2011 and amends the Planning and Compulsory Purchase Act 2004. It places a legal duty on local planning authorities, county councils in England and public bodies to engage constructively, actively and on an on-going basis to maximise the effectiveness of Local Plan preparation in the context of strategic cross boundary matters.

The Coventry Local Plan identifies safeguarded land that relates to Green Belt parcels that straddle administrative boundaries. These parcels, when considered in isolation, through the Coventry Local Plan are unsuitable for removal from the Green Belt due to the inability to create defensible boundaries that would endure beyond the plan period. However, when considered alongside development options within Warwick District the land could provide longer term possibilities for the release of land from the Green Belt to support cross-boundary development proposals in Warwick District.

The Council cannot place undue pressure on neighbouring authorities to develop on specific sites but safeguarded land provides a mechanism that responds to longer-term development options which means our own plan remains flexible and able to respond to emerging circumstances in neighbouring areas. This enables us to continue to discharge our responsibilities through the duty to co-operate. If these sites do not come forward through cross-boundary designations then the land is considered for formal return to Green Belt.

Policy DS2 states that Coventry City Council will work with neighbouring authorities within its Housing Market Area to support the delivery of the development needs identified in Policy DS1 that originate from the city. It further states that Where sites cross or are adjacent to administrative boundaries and are not subject to joint development plan documents, the Council will continue to work proactively and on an on-going basis with all relevant partners to enable the delivery of new development on these sites.

Policy GB2 states:

1. The areas of Safeguarded Land proposed partly or wholly comprise the following sites and are shown on the Policies Map.
 - a) Land south of Westwood Heath Road;
 - b) Land south of Bishop Ullathorne School;
 - c) Playing Field south of Finham Park School; and
 - d) Land west of Finham Primary School.

Any development of these sites will be subject to consideration through a full or partial review of this Local Plan having explicit regard to development proposals in Warwick District.

Furthermore, paragraph 139 of the National Planning Policy Framework states that "When defining Green Belt boundaries, plans should:

- c) where necessary, identify areas of safeguarded land between the urban area and the Green Belt, in order to meet longer-term development needs stretching well beyond the plan period;
- d) make clear that the safeguarded land is not allocated for development at the present time. Planning permission for the permanent development of safeguarded land should only be granted following an update to a plan which proposes the development".

Development is defined under the 1990 Town and Country Planning Act as "the carrying out of building, engineering, mining or other operation in, on, over or under land, or the making of any material change in the use of any building or other land." Most forms of development require planning permission". Clearly, the proposal constitutes development under this definition.

The proposal is considered as prejudicial because any development within the safeguarded area would prevent the entire area being considered together as a strategic site for future development needs for both Coventry and Warwick. Therefore, there is a particular need to ensure that the site, as a whole, is safeguarded from development. Allowing the land to be developed would prejudice the strategic nature of the policy's aim to protect safeguarded land for future cross-boundary needs and possibly trigger an early review Warwick District Council's Local Plan.

Moreover, Coventry City Council's Local Plan is not yet up for review, so any release of land within land designated GB2, particularly for non-strategic purposes, would be inappropriate and considered premature.

The proposals are therefore contrary to Policy GB2 and the principle of development is not considered to be acceptable.

Impact on Neighbouring Amenity

It is considered that the quiet nature of the proposal and the distance from neighbouring properties would have no detrimental harm to the living conditions of the neighbouring properties.

Design and Appearance

Section 12 of the National Planning Policy Framework 2019 (NPPF) outlines the Government's commitment to good design and attaches great importance to the design of the built environment, highlighting it as a key aspect of sustainable development. Decisions should not attempt to impose architectural styles or particular tastes and they should not stifle innovation, originality or initiative; however new developments should seek to promote or reinforce local distinctiveness. Decisions should address the connections between people and places and the integration of a new development into its existing environment. Consequently, decisions should aim to ensure that developments are visually attractive as a result of good architecture and appropriate landscaping.

The NPPF further states that "where the design of a development accords with clear expectations in plan policies, design should not be used by the decision-maker as a valid reason to object to development". However, "Permission should be refused for development of poor design that fails to take the opportunities available for improving the character and quality of an area and the way it functions, taking into account any local design standards or style guides in plans or supplementary planning documents" (130).

Policy DE1 of Coventry City Council's Local Plan (CLP) states that "All development proposals must respect and enhance their surroundings and positively contribute towards the local identity and character of an area".

It is considered that the proposed temporary building, layout and the soft and hard landscaping treatment would not have any detrimental impact on the street scene or in design terms. However, the site is not located in a suitable location as the site is to be protected from development until a later date when the current local plan is to be reviewed.

The option of a temporary use of the site has been considered with the Planning Policy Officer but it was felt that a temporary proposal would still be unacceptable given the strategic purpose of the designation. Even a temporary permission would fetter the strategic purpose of the policy as it could seriously jeopardise the entire area being considered together to deliver the future needs of both Coventry and Warwick.

Any premature release, even temporary, could trigger WDC to review their local plan sooner than anticipated, which would then have implications for Coventry's future needs.

Highway Safety

Policy AC1 refers to developments which will create additional trips on the transport network, whilst AC3 requires safe and appropriate access to the highway system together with satisfactory on site arrangements for vehicle manoeuvring so as to ensure safety for all users and AC4 encourages walking and cycling.

Access to the application site is via Featherbed Lane, a narrow access road approximately four metres wide to the gateway of No.2 Featherbed Lane, after which it narrows to less than three metres in width. Within the application site there will be 10 car parking spaces, one for staff and nine for disabled persons. Additional parking will be available at St John's Church.

Coventry City Council Highways were consulted and raised no objection, stating *"In respect of this application the Highway Authority considers that the impacts of the development are not severe and has no objections to the current proposal"*.

Equality Implications

Section 149 of the Equality Act 2010 created the public sector equality duty. Section 149 states:-

(1) A public authority must, in the exercise of its functions, have due regard to the need to:

- a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
- b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

Officers have taken this into account and given due regard to this statutory duty, and the matters specified in Section 149 of the Equality Act 2010 in the determination of this application.

There are no known equality implications arising directly from this development.

CONCLUSION

It is therefore recommended that planning permission be refused.

REASON FOR REFUSAL

Whilst the land is not within the Green Belt, it has been set aside as safeguarded land for potential development needs in the future and is therefore not allocated for development within the current Coventry Local Plan. Any development of this site is contrary to Policies DS2 and GB2 and the principles of the National Planning Policy Framework (paragraph 139), as it aims to ensure flexibility in realising cross-boundary opportunities to facilitate development in the longer term. To allow development at this stage would be premature and contrary to the principles of Policies DS2 and GB2 and the need for safeguarding land, thus undermining the Local Plan process for both Coventry and Warwick District Councils and the need to support sustainable development, the on-going commitment to discharge the duty to co-operate and the long term protection and management of the Green Belt.

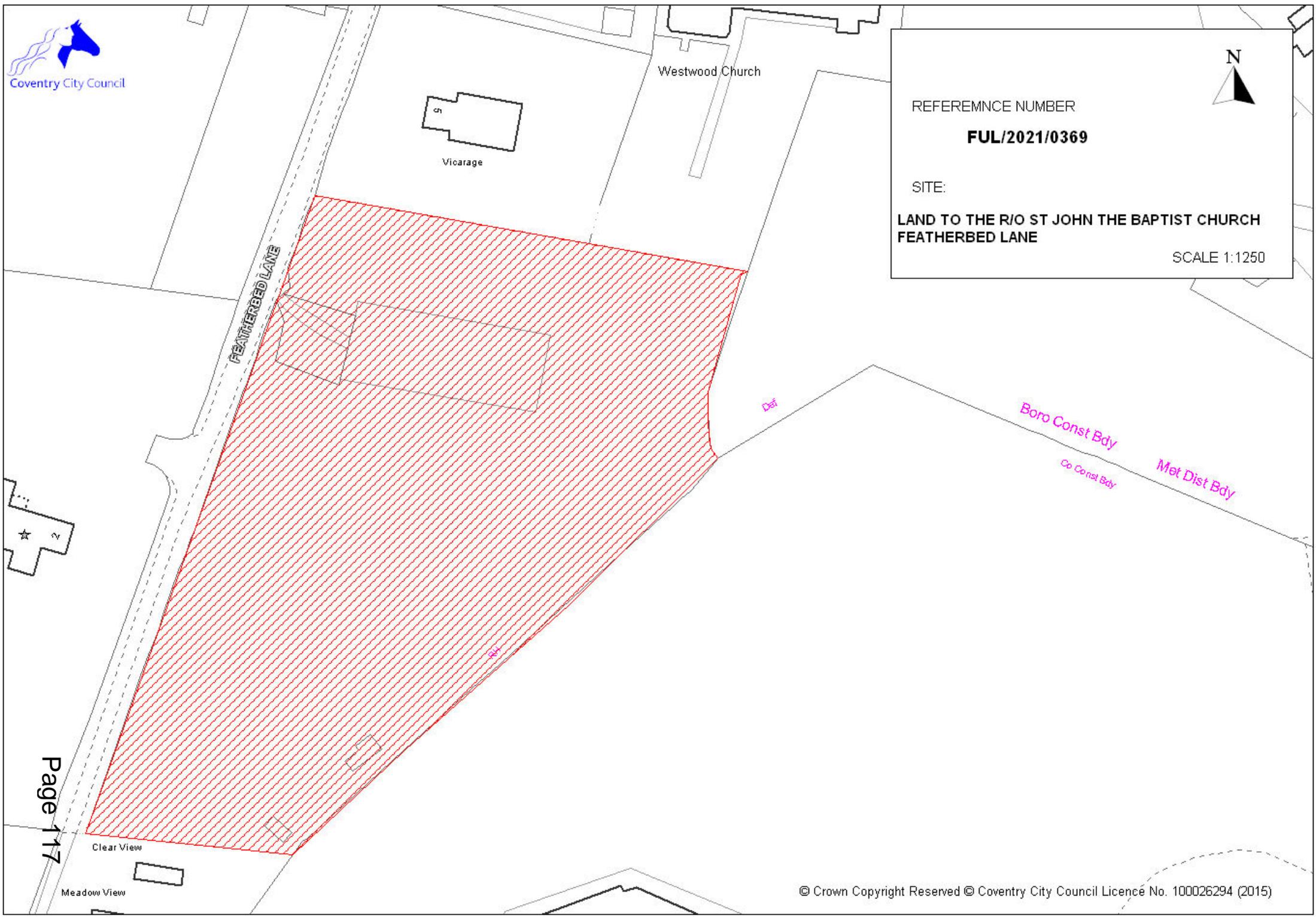
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REFEREMNCE NUMBER
FUL/2021/0369

SITE:
**LAND TO THE R/O ST JOHN THE BAPTIST CHURCH
FEATHERBED LANE**

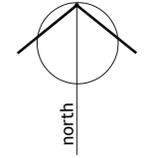
SCALE 1:1250





KEY:

-  Fences
-  Coventry City limits
-  Proposals boundary
-  Ownership boundary



SUITABLE FOR PLANNING PURPOSES ONLY

A: Ownership boundary added	JB	02-02-21
REV	Drawn	Date

CLIENT

ST. JOHN'S WESTWOOD

J B Landscape Associates
25 VICTORIA STREET, WARWICK CV34 4JT

PROJECT

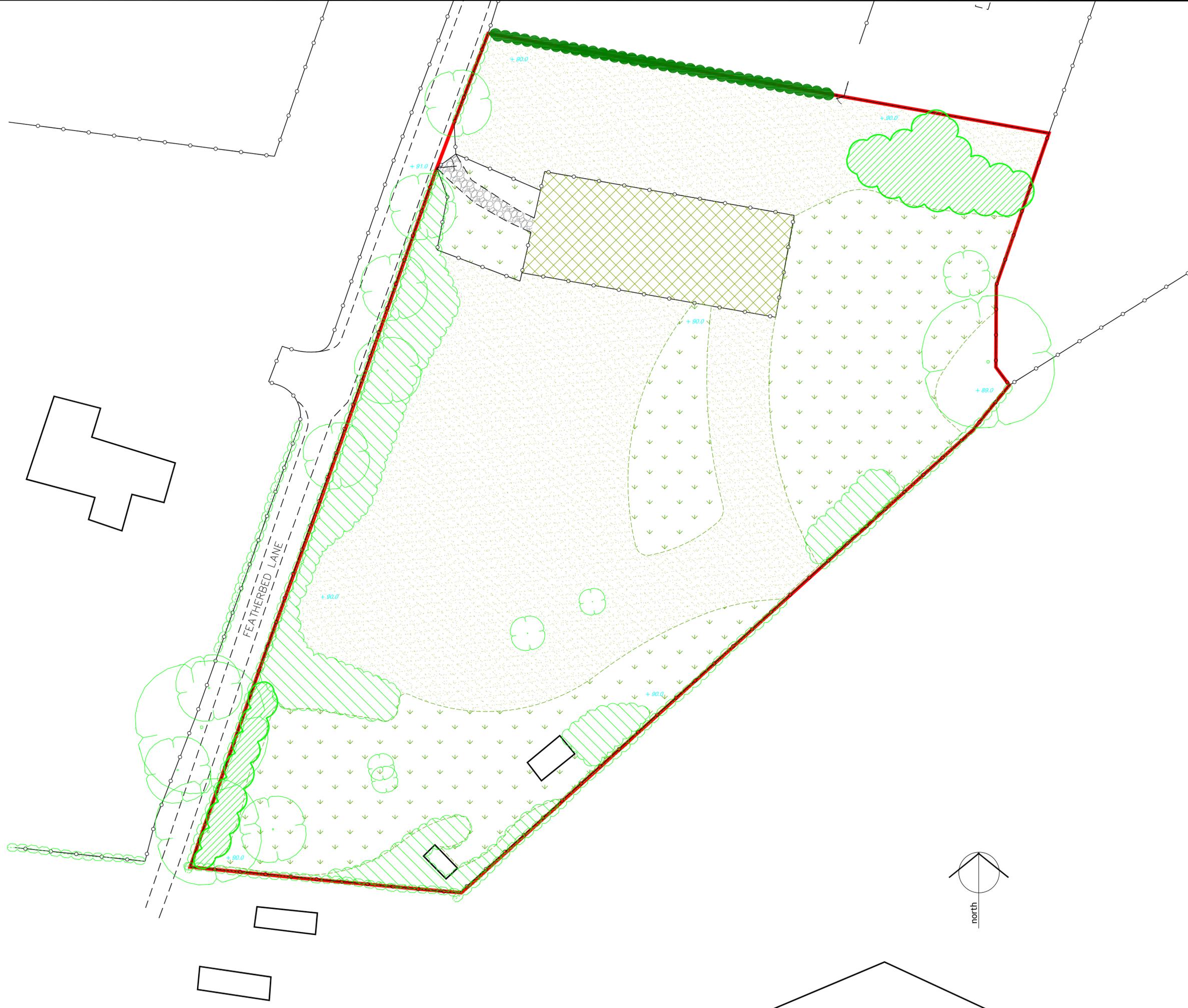
THE GARDEN OF HOPE

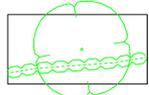
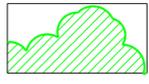
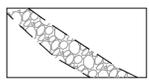
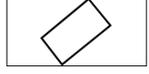
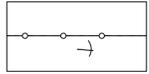
DRAWING

SITE LOCATION & CONTEXT

DRAWN	JB	CHECKED	JS	APPROVED	JB
SCALE		1:500@A1		DATE	22 Dec 2020

DRAWING No	2148 - PL001	REV	A
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- KEY:**
-  Existing hedgerows & trees to be retained
 -  Existing evergreen hedgerow
 -  Existing tree group
 -  Existing scrub
 -  Existing meadow areas
 -  Areas of cut grass
 -  Reinforced grass area (overflow church car park)
 -  Rough gravel track
 -  Timber Huts
 -  Fences and gates
 -  Approximate existing spot levels
 -  Site boundary

SUITABLE FOR PLANNING PURPOSES ONLY

REV	Drawn	Date
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CLIENT

ST. JOHN'S WESTWOOD

J B Landscape Associates
 25 VICTORIA STREET, WARWICK CV34 4JT

PROJECT

THE GARDEN OF HOPE

DRAWING

EXISTING SITE

DRAWN	JB	CHECKED	JS	APPROVED	JB
SCALE		1:250@A1		DATE	
		22 Dec 2020			

DRAWING No	REV
2148 - PL002	

PLANT SCHEDULE

Trees	Size	Nos
AC	Acer campestre 2.1-2.4m high FTHD	2
BP	Betula pendula 2.1-2.4m high FTHD	2
QR	Quercus robur 2.1-2.4m high FTHD	6

All trees to receive rabbit guards and shall be planted and secured according to the Landscape Specification. Mulch tree stations with 75mm depth bark mulch.

NATIVE HEDGE PLANTING (nh)
To be planted in a double staggered row of 5 plants per linear metre within gaps in existing hedging or where shown. Plants to be inserted into ground in random groups of 7-11 by species along length of proposed hedge. Plant into 1m wide biodegradable mulch mat roll or mulch with 75mm deep bark mulch.

Size	Nos
Acer campestre 10% 1+1 60-90 BR	
Corylus avellana 15% 1+1 60-90 BR	
Crataegus monogyna 30% 1+1 60-90 BR	
Ligustrum vulgare 20% 3L pots	
Prunus spinosa 10% 1+1 60-90 BR	
Rosa canina 5% 1+1 60-90 BR	
Viburnum opulus 10% 1+1 60-90 BR	

NATIVE WOODLAND PLANTING
To be planted at 1.5m centres in random groups of 9-13no. by species (except Rosa and Quercus which are to be planted individually). Planting shall consist of the following percentages:

Size(cm)	
Betula pendula 5%	1 + 1 60-90 BR
Corylus avellana 10%	1 + 1 80-100 BR
Crataegus monogyna 20%	1 + 1 80-100 BR
Malus sylvestris 10%	1 + 1 60-90 BR
Rosa canina 5%	2L POTS
Prunus avium 10%	1 + 1 80-100 BR
Prunus spinosa 20%	1 + 1 60-90 BR
Quercus robur 5%	1 + 1 60-90 BR
Taxus baccata 10%	1 + 1 40-60 FTHD CG
Viburnum lantana 5%	1 + 1 60-90 BR

All whips and transplants supplied to site shall be of British provenance. A certificate proving such shall be provided to the Landscape Architect prior to delivery to site. All whips to have clear plastic spiral rabbit guards, secured into ground with bamboo canes.
Roots of all bareroot stock shall be treated with Mycorrhizae Whip Dip to manufacturers instruction. Supplier Burrow Nursery on 01435 862992.

ORNAMENTAL SHRUBS etc	Size (cm)	Rate
AM Alchemilla mollis	2L POT	7/m ²
AG Allium 'Globemaster' or similar	Bulbs	5/m ²
AV Aubretia varieties	2L POT	7/m ²
BS Buxus sempervirens	3L 40-60	4/m ²
COE Carex oshimensis 'Evergold'	2L POT	7/m ²
CTR Ceanothus thyrsiflorus repens	3L 40-60	4/m ²
CT Cerastium tomentosum	2L POT	7/m ²
CxP Cytisus x praecox	3L 40-60	3/m ²
CL Crocosmia Lucifer	Corms	5/m ²
DC Deschampsia caespitosa	2L POT	7/m ²
ESQ Euonymus 'Silver Queen'	3L 30-40	4/m ²
GH Genista hispanica	3L 20-30	4/m ²
GL Genista lydia	3L 30-40	4/m ²
GG Geranium grandiflorum	3L 20-30	5/m ²
LH Lavandula 'Hidcote'	3L 40-60	4/m ²
LM Lonicera 'Maygreen'	3L 40-60	3/m ²
MSC Miscanthus sinensis condensatus	2L POT	7/m ²
ROP Rosmarinus officinalis prostrata	3L 40-60	4/m ²

CLIMBERS for pergola	Size	specimen
CMR Clematis montana 'Rubens'	5L 60-90	specimen
CB Clematis balearica	5L 60-90	specimen
JOG Jasminum officinale Grandiflorum	5L 60-90	specimen
LP Lonicera periclymenum	5L 60-90	specimen
LT Lonicera tellmaniana	5L 60-90	specimen
WS Wisteria sinensis	5L 60-90	specimen

BULBS in grass	Size	Rate
NC Narcissus 'Carlton'	Bulbs	15/m ²
CV Crocus mixed varieties	Bulbs	25/m ²

ORNAMENTAL HEDGE for maze
BS Buxus sempervirens 5L 40-60 7/linear m.
Box to be planted in a double staggered row in a 700mm wide bed.

All ornamental shrub & hedge plants shall be planted into 300mm of good quality site-won topsoil, and cultivated to provide good even tilth free from rubbish, large stones and clods.
All ornamental shrub & shrub areas shall receive 50mm deep washed river pebbles as a mulch.

NOTES
All works shall be in accordance with the Landscape Specification and comply with current Health & Safety and CDM Regulations.
No trees shall be planted within 5m of drains or services without the installation of a root barrier. The Landscape Contractor shall be responsible for acquiring 'as-built' drain and service plans before commencing work on site.
Where existing grass is disturbed it shall be reinstated to match surrounding levels and made good.
Where plants nos. or areas are shown on a schedule, these are for assistance only and do not over-ride actual nos. or areas on the drawing.
This drawing and design is the property of J B Landscape Associates. Copyright is reserved by them and no part or whole of this drawing may be reproduced without the consent in writing of J B Landscape Associates. Contractors must verify all dimensions on site before commencing work. Do not scale from this drawing. This drawing was computer generated using AutoCad.



TYPICAL GARDEN ROOM 6.0m x 3.0m x 2.5m ht.
(for detailed description see separate sheet)

POND
The POND is to be dug 1.2 metres deep and lined with Visqueen pond liner or similar approved, ensuring the lining extends 1.0m beyond the pond edges, covering the pond bed and extended edges with 300mm topsoil and occasional large stones/boulders around the perimeter.

SENSORY PATH
The SENSORY PATH is to be 1.2m wide and edged in 100mm x 25mm timber edging nailed to 150 x 50mm sq pegs. Surface finishes shall be a nominal 75mm depth and vary between 1.0m and 3.0m in length; materials may include pea shingle, sharp sand, timber, sawdust, bark chips, crushed stone, river pebbles, brick paviours and artificial grass; all laid over a geotextile membrane on 150mm depth blinded Type 1 crushed stone base, laid to gentle falls and compacted.

PERGOLA
The pergola shall be 2.4m high x 1.7m width (approx) between upright centres, and constructed in approved fully impregnated softwood. Uprights shall be planed Douglas Fir, ex 125mm sq. section with 4-way weathered top and set into concrete of 30N/mm² strength to a depth of 0.8m. Horizontal purlins and rafters shall be 150 x 50mm section s/w, rafters 2.4m long with shaped ends, notched onto purlins by 60mm; and purlins coach-screwed to the uprights at a clearance height of 2.0m. Use 2no. zinc coach-screws per joint. All timber shall be treated with 2 coats of Sadolin or Cuprinol stain.

CROSS
The CROSS is to be constructed in the same materials as the pergola, with a 1.8m x 200 x 125mm horizontal, coach-bolted to a notched 3.6m long x 200 x 200mm upright, set by 800mm depth into a 900 x 600 x 600mm concrete foundation



KEY:

- Existing hedgerows & trees to be retained
- Existing evergreen hedgerow
- Existing tree group retained
- Existing scrub retained
- Existing meadow areas retained
- Areas of short mown grass formed by frequent close-cutting of existing meadow areas
- Reinforced grass surfacing - existing area extended
- Proposed Box hedging
- Proposed native hedge
- Proposed trees
- Proposed native woodland
- Proposed ornamental planting
- Proposed bulbs in grass
- Proposed timber-edged 50mm sealed gravel surfacing on 150mm depth Type 1 base
- Existing rough gravel track
- Proposed timber bollards
- 800mm height timber raised beds with base drainage gravel
- Existing Fences and gates
- Approximate existing spot levels retained
- Proposals boundary

SUITABLE FOR PLANNING PURPOSES ONLY

B: Parking revised	JB	04-02-21
A: Revised to client's requests	JB	26-01-21
REV	Drawn	Date

CLIENT
ST. JOHN'S WESTWOOD

J B Landscape Associates
25 VICTORIA STREET, WARWICK CV34 4JT

PROJECT
THE GARDEN OF HOPE

DRAWING
PROPOSED LAYOUT

DRAWN	JB	CHECKED	JS	APPROVED	JB
SCALE		1:250@A1		DATE	
DRAWING No					REV
2148 - PL003					B

Planning Committee Report	
Planning Ref:	TELO/2021/0198
Site:	Land at Scots Lane
Ward:	Bablake
Proposal:	Prior approval for the proposed installation of a 17.5m high monopole together with the installation of ground-based equipment cabinets and ancillary development
Case Officer:	Shamim Chowdhury

SUMMARY

The prior approval seeks to install a 17.5m monopole mast and associated equipment cabinets. As per Schedule 2, Part 16, Class A of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (GPDO) certain telecommunications development such as masts and equipment cabinets are permitted development subject to the local planning authority approving siting, design and appearance.

BACKGROUND

The proposed installation is part of Vodafone's network upgrade to rollout 5G network in the area. The proposal has amended and reduced the mast height from 20m to 17.5m in order to minimise visual impact. The proposed mast and equipment cabinets would be shared with Telefonica (O2 network).

KEY FACTS

Reason for report to committee:	Cllr Williams called in
Current use of site:	Highway grass verge
Proposed use of site:	Telecommunication Infrastructure

RECOMMENDATION

Planning committee are recommended to grant Prior Approval subject to compliance with the submitted drawings.

REASON FOR DECISION

- The proposal is acceptable in principle.
- The proposal will have limited adverse impact upon the visual amenity of the area; however, the social and economic benefits of the proposal would outweigh visual harm.
- The proposal falls within the criteria of Schedule 2, Part 16, Class A of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (GPDO) and accords with the NPPF.

BACKGROUND

APPLICATION PROPOSAL

The application seeks prior approval for the erection of a 17.5m high mono-pole mast and associated equipment cabinets as part of 5G network rollout by Vodafone. There are three equipment cabinets in association with the proposed mono pole mast as ancillary development. Two of the equipment cabinets measure in floor area 1.6m X 0.8m (1.44m²) and 0.7m X 0.8m (0.56m²) with a height of 1.6m and the other cabinet is fairly small in floor area with a height of 1m. The height of the proposed monopole has been amended and reduced from 20m to 17.5m to minimise impact on the visual amenity of the street scene.

SITE DESCRIPTION

The site is a grass verge along the west side of Scots Lane around 22m north of Hollyfast Road junction. There is footpath along the grass verge and beyond which is Bablake School Playground. There is a palisade fence separating the playing field from the public footpath and grass verge. There is a mature tree line on the edge of the playground along the public footpath and one mature tree on the grass verge adjacent to the proposed mast. The site is opposite Coundon reservoir and away from residential properties. There are residential properties to the north-east and south-west of the application site.

PLANNING HISTORY

There are no historic planning applications on this site.

POLICY

National Policy Guidance

National Planning Policy Framework (NPPF). The NPPF sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that is relevant, proportionate and necessary to do so.

The National Planning Practice Guidance (NPPG) adds further context to the NPPF and it is intended that the two documents are read together.

Local Policy Guidance

The current local policy is provided within the Coventry Local Plan 2016, which was adopted by Coventry City Council on 6th December 2017. Relevant policy relating to this application is:

Policy DE1 Ensuring High Quality Design
Policy C1 Telecommunications

Supplementary Planning Guidance/ Documents (SPG/ SPD):

SPG Telecommunications – A Design Guide, September 2005

CONSULTATION

No Objections received from:

- Highways
- Environmental Protection

Three site notices were posted on 14th February 2021.

Four letters of objection including one from former Councillor Glenn Williams have been received, raising the following material planning considerations:

- a) The proposed mast is close to an existing mast
- b) The proposed mast would harm the open landscape area
- c) Would appear unsightly and harm the visual amenity of the street scene.
- d) The siting of the mast proximity to two primary schools and a school playing field
- e) There are already three masts in the area why another one is required.

Any further comments received will be reported within late representations.

APPRAISAL

The proposals are permitted development under Schedule 2, Part 16, Class A of the GPDO. Therefore, the main issues in determining this application are siting and design only, as matters for consideration for prior approval.

Siting, Design and Appearance

The NPPF recognises that advanced, high quality and reliable communications infrastructure is essential for economic growth and social well-being. Planning policies and decisions should support the expansion of electronic communications networks, including next generation mobile technology (such as 5G). Nevertheless, paragraph 113 of the NPPF suggests that the number of masts should be kept to a minimum and where new sites are required (such as for new 5G networks), equipment should be sympathetically designed and camouflaged where appropriate.

Paragraph 115 of the NPPG suggests that Applications for electronic communications development (including applications for prior approval under the General Permitted Development Order) should be supported by the necessary evidence to justify the proposed development. This should include:

- a) the outcome of consultations with organisations with an interest in the proposed development, in particular with the relevant body where a mast is to be installed near a school or college, or and
- b) for an addition to an existing mast or base station, a statement that self-certifies that the cumulative exposure, when operational, will not exceed International Commission guidelines on non-ionising radiation protection; or
- c) for a new mast or base station, evidence that the applicant has explored the possibility of erecting antennas on an existing building, mast or other structure and a statement that self-certifies that, when operational, International Commission guidelines will be met.

Policy DE1 of the Coventry Local Plan 2016 seeks to ensure that development complements or enhances the character of the surrounding area.

Policy C2 related to Telecommunication on Coventry Local Plan 2016 states that when considering notifications, planning applications and prior approval applications, regard will be given to the following factors:

- a) operational requirements of the telecommunication networks and the technical limitations of the technology, including any technical constraints on the location of telecommunications apparatus;

- b) the need for the ICNIRP Guidelines (and any other relevant guidance in place at the time of the application) for safe emissions to be met;
- c) the potential for sharing existing masts, buildings and other structures; and
- d) the impact of the development on its surroundings with particular regard to the following criteria:
 - i. the visual amenity, character or appearance of the surrounding area.
 - ii. apparatus and associated structures sited on a building should be sited and designed in order to seek to minimise impact to the external appearance of the host building.
 - iii. development should not have an unacceptable effect on conservation areas or buildings of architectural or historic interest or areas of ecological interest or areas of landscape value or sites of archaeological importance.
 - iv. the proposed provision of landscaping.

The Council's SPG "Telecommunications – A Design Guide" states that within a residential area monopole masts, street works type masts and antenna on buildings may be acceptable provided that they are sited carefully and designed in relation to dwellings and the street scene. Whilst the Council recognises that some elements of this SPG are out of date with regards legislation, the criteria, nonetheless, used in the assessment of prior notification submissions are still relevant, as acknowledged by the Local Plan Inspector when the SPG was carried forward to the new Local Plan.

The SPG states that "Monopole masts may be acceptable within such areas provided that they will:

- Demonstrate that no alternative sites are available;
- Not be visually prominent in the street scene or from dwellings due to height and/or design structures;
- Be screened from public areas by buildings, structures or vegetation;
- Not be more than 2 metres higher than their visual scree; and
- Be colour coated to reduce visual prominent (including equipment cabinets and security fencing).

The application site which is a grass verge between public footpath and carriageway is fairly open and on a landscape setting. There are no buildings close to the site and a large area of playing fields to the west and concealed reservoir ground to the east. A mature tree line within the field provides screening. There is a mature tree on the grass verge to the south of the site nearer to the junction of Hollyfast Road and Scots Lane. The proposed mast would sit on its own around 13m away from the tree which is on the grass verge and in the backdrop of trees which are on the playing field. The view of the mast from Hollyfast Road and from the surrounding area will be less prominent as it would be seen in the backdrop of the mature trees. Due to the siting and road layout of Scots Lane adjacent to the proposed site, which is fairly open and a straight stretch of road, the view from Scots Lane will be more striking in particular to pedestrians and motorists. This view would be less striking to the motorists from south to north direction due to the existing mature tree screening the mast partially. The proposed mast would be away from the direct visibility line of the surrounding residential properties.

There is an existing telecommunications street pole at Westhill Road used by two operators, but this mast only provides 4G coverage. In addition, there is one EE 4G mast on the Coundon Reservoir to the east of the proposed site. The applicant states that this site is to the far east of the existing installation on Westhill Road to replicate coverage.

Moreover, the base station would require significant redevelopment to accommodate operators' (Vodafone) equipment. With regards to the existing mast on Westhill Road, the applicant states that there is no comparable street works solution that can accommodate 5G antennas for both Vodafone and Telefonica. It would require significant redevelopment and a completely new mast of substantial width and height, as well as taking up the majority of land at its base. Therefore, the applicant considered that redevelopment would be inappropriate within the streetscene, compromise pedestrian movements and impact on highway safety. The existing mast on Bablake Playing Fields close to Norman Place cannot provide coverage to Scots Lane as it is too far away.

The proposed mast and equipment cabinets are to create a new base to provide 5G coverage. The applicant indicates that a new mast and base is required due to technology involved in providing 5G network coverage. It is also operator's (Vodafone) licence obligations to upgrade and enhance their network services to deliver new 5G technologies in this area. In addition, the proposed monopole and cabinets would be shared by other telecommunications operators (Vodafone and Telephonica) which would help to reduce the proliferation of electronic communication masts.

Negotiations have been sought to reduce the height of the mast further, however it is understood that certain height is essential for the requirements of 5G installation and the applicant insists that the height of the proposed development must be sufficient to enable local provision and clear surrounding buildings and trees. The applicant also demonstrated that the other existing sites were considered as site selection strategy to minimise environmental impact and established that the current site for a new mast and base station is the most viable option in this area.

It is acknowledged that the mast will be 2.5 metres higher than the preferred height set out in the Council's Telecommunications SPG and also due to its siting within an open roadside location would have some impact on the visual amenity of the street scene. However, this impact needs to be assessed against the public benefits of the proposal. The introduction of 5G coverage in the area will contribute to delivering a modern, advanced, high quality and reliable communications infrastructure that supports a range of consumers, including the emergency services. It follows, then, that the delivery of such high-end technology will support economic growth and the local community by enabling fast and reliable communication to take place, for example by helping people gain employment, access services, support their health and well-being, whilst also assisting new technologies. The proposal would also support the Governments aims regarding 'levelling up' digital connectivity.

The applicant has provided a Declaration of Conformity with ICNIRP Public Exposure Guidelines, ensuring that the proposed telecommunications equipment will be in full compliance with the requirements of the radio frequency (RF) public exposure guidelines of the International Commission on Non-Ionising Radiation Protection (ICNIRP).

Whilst it is acknowledged that the proposal does not strictly accord with the Telecommunications SPG, given the characteristics of the site and surrounding location, it is not considered that the proposal will result in serious harm to the visual amenity of the street scene or the character of the area.

The proposal will improve 5G coverage in this area, and the NPPF seeks to support high quality communications, and therefore the limited harm which results from the development is outweighed by the social and economic benefits which would be brought by the proposed development in the area.

The applicant confirms that the mast and cabinets will be coloured RAL 9005 Black to blend in with the existing street furniture and minimise the impact upon the urban streetscapes.

Equality Implications

Section 149 of the Equality Act 2010 created the public sector equality duty. Section 149 states:-

(1) A public authority must, in the exercise of its functions, have due regard to the need to:

- a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
- b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

Officers have taken this into account and given due regard to this statutory duty, and the matters specified in Section 149 of the Equality Act 2010 in the determination of this application. There are no known equality implications arising directly from this development.

Conclusion

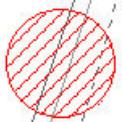
The proposal falls within the provisions of Schedule 2, Part 16, Class A of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (GPDO) and in line with guidance of the NPPF and therefore it is recommended that the prior approval is not required and those details, having been submitted, are acceptable.



REFERENCE NUMBER
TELO/2021/0198

SITE:
**LAND AT COUNDON RESERVOIR
SCOTS LANE**

SCALE 1:1250



115.5m

Pump House

Mast (Telecommunication)

LITTLE COUNDON
CLOSE

JOHN MURPHY

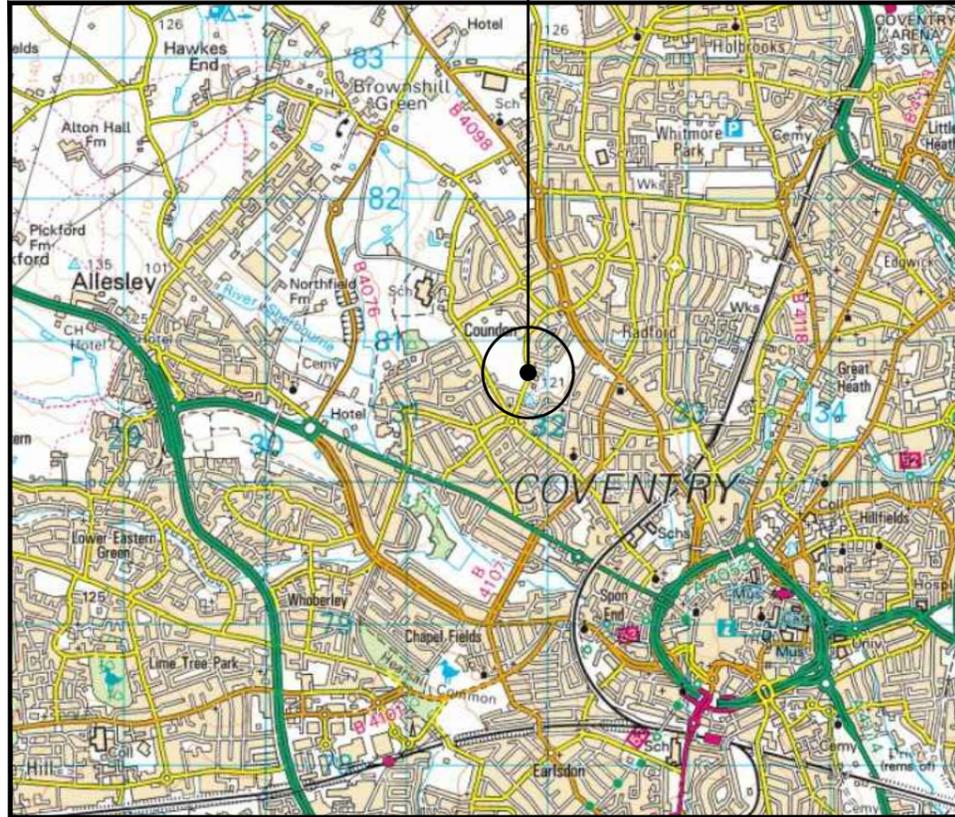
RESERVOIR WALK

PEOPLE ROAD

Reservoir (covered)

Path

SITE LOCATION



0 500 1000 1500 2000 2500m
1:50000

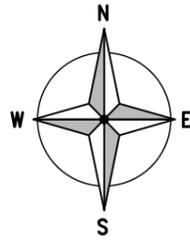
SITE LOCATION
(Scale 1:50000)

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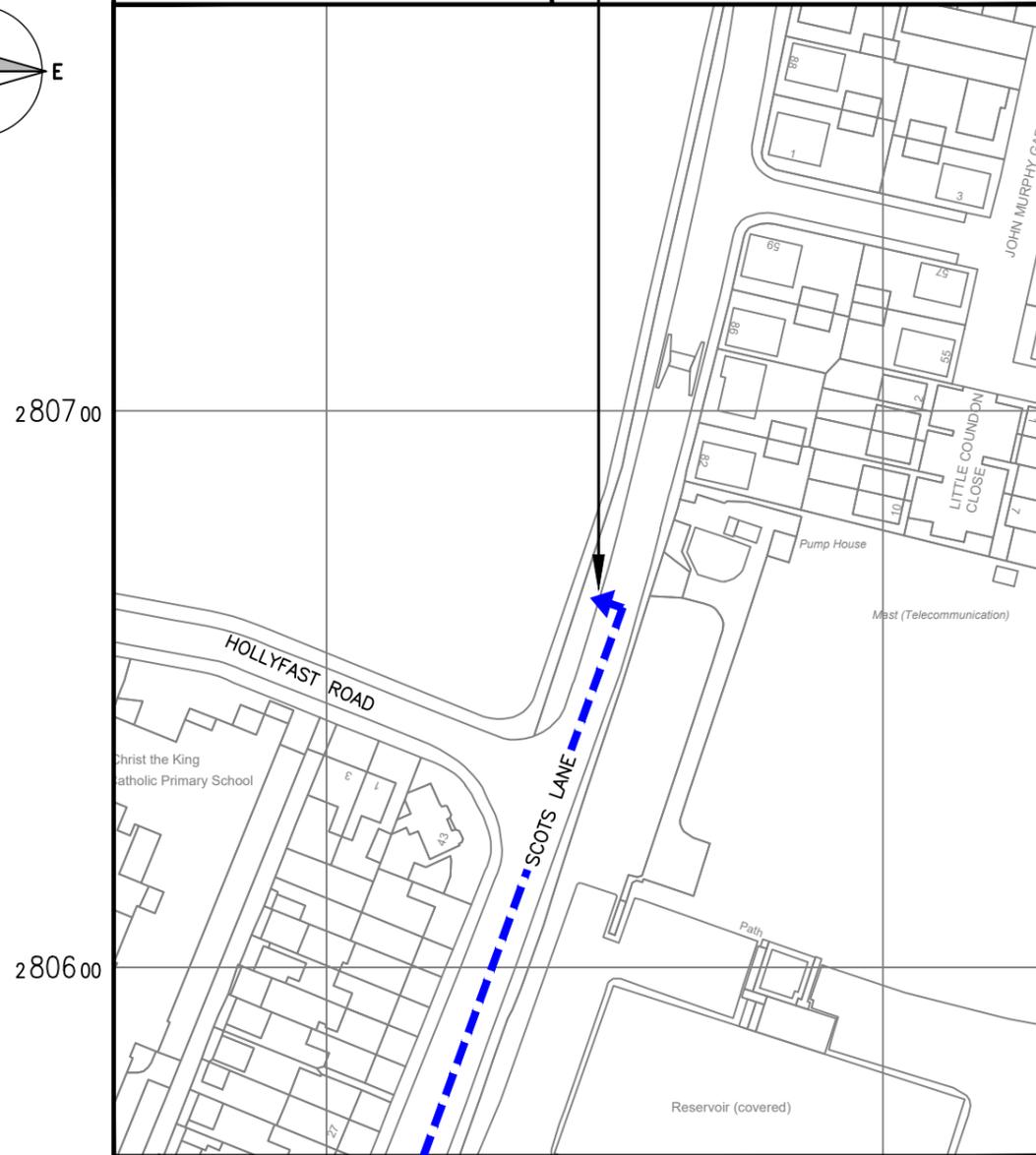
SITE PHOTOGRAPH

The drawings comply with VF Non-Standard ICNIRP guidelines. Designed in accordance with Cornerstone documents: SDN0009.



← - - - = RIGHT OF ACCESS
← - - - = PUBLIC ACCESS ROUTE

SITE LOCATION



280700

280600

431800

431900

0 12.5 25 37.5 50 62.5m
1:1250

DETAILED SITE LOCATION
(Scale 1:1250)

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ALL DIMENSIONS ARE IN mm UNLESS NOTED OTHERWISE

N.G.R E: 431848 N: 280671

DIRECTIONS TO SITE:
HEAD WEST ON BIRMINGHAM RD/A45 TOWARD PICKFORD GREEN LN. FOLLOW PICKFORD GREEN LN, UPPER EASTERN GREEN LN AND PARKHILL DR TO PICKFORD WAY/A4114 IN COVENTRY. FOLLOW A4114 TO SOUTHBANK ROAD. CONTINUE ON SOUTHBANK RD DRIVE TO SCOTS LANE. THE SITE IS ON THE LEFT HAND SIDE.

NOTES:

A	Issued for Approval	CDN	ND	18.12.20
REV	MODIFICATION	BY	CH	DATE

WHP Telecoms Ltd
Faraday Court
401 Faraday Street
Birchwood Park
Warrington WA3 6GA
Tel: 01925 424100 Fax: 01925 424101
e-mail: info@whptelecoms.com

vodafone
CORNERSTONE - BEACON

Cell Name		Opt.
SW ON SCOTS LANE		E
Cell ID No		
CORNERSTONE	TEF	VF
30247800	-	18968

Site Address / Contact Details
SCOTS LANE
COUNDON
COVENTRY
WARWICKSHIRE
CV6 2DQ

Drawing Title: SITE LOCATION MAPS			
Purpose of issue: PLANNING			Dwg Rev:
Drawing Number: 100			A
Surveyed By: SD	Original Sheet Size: A3		Pack Issue:
Drawn: CDN	Date: 18.12.20	Checked: ND	Date: 18.12.20
			B

Planning Ref:	FUL/2020/1334
Site:	Garages, Overdale Road, Coventry, CV5 8AL
Ward:	Whoberley
Proposed Number of Units:	Two
Proposed Number of Affordable Housing Units:	Two – Development by Citizen Housing (Social Housing Provider)
Proposal:	Demolition of existing garages and development of 2 x 2-bed bungalows
Case Officer:	Fiona Runacre

SUMMARY

The site comprises 7 garages and 13 parking spaces, located in a predominantly residential area. Access is afforded from Overdale Road. A pair of semi-detached bungalows with associated parking for the dwellings and landscaping is proposed, following demolition of the existing garages.

KEY FACTS

Reason for report to committee:	More than 5 representations objecting to the application
Current use of site:	Garages and parking
Proposed use of site:	Residential properties with associated parking

RECOMMENDATION

Planning committee are recommended to grant planning permission subject to conditions.

REASON FOR DECISION

- The proposal is acceptable in principle and will deliver additional affordable housing stock.
- There is capacity within the area to absorb the displaced parking.
- The proposal will not adversely impact upon highway safety.
- The proposal will not adversely impact upon the amenity of neighbours.
- The proposal accords with Policies: DS3, DE1, H3 and AC1, AC2, AC3 and AC4 the Coventry Local Plan 2016, together with the aims of the NPPF to deliver housing in a sustainable location, make use of previously developed land and secure high-quality design.

BACKGROUND

APPLICATION PROPOSAL

It is proposed to erect one pair of semi-detached bungalows on the site, following demolition of the existing garages. The dwellings would be set back within the site, having a frontage building line to match that of the adjacent property no. 52 Overdale Road, which is a two-storey dwelling. Four parking spaces would be provided fronting the bungalows; one for each property plus two visitor spaces.

The dwellings to be provided on the site will assist in meeting the current shortfall of social housing units within the City. The redevelopment of the site is part of Citizens'

Housing review and redevelopment of underused parcels of land, such as garage courts, to deliver needed affordable housing.

SITE DESCRIPTION

Currently the site provides 7 garages and 13 car parking spaces, primarily for residents, although there is no physical restriction on access to the parking spaces. The application site is adjoined by residential properties to the north, south and west of the application site. The property immediately to the east is No. 294 Greendale Road, which is a single storey flat roofed dwelling. To the south are two storey properties. Those properties immediately to the north are single storey flat roofed dwellings and their rear gardens back onto the footpath adjacent to the application site, (Nos. 56-60 Overdale Road). The footpath adjoining the northern boundary of the site provides pedestrian access from Overdale Road to Greendale Road, onto which No.s 294-302 Greendale Road front.

The existing 7 garages appear to be in good condition, 4 of which are in use, as is the car parking area with 13 spaces.

The site is level, adjacent to rising ground to the south, with properties to the north being sited at a lower level due to the local topography. There is a change in levels from the north to south boundary of c. 1.8 m.

PLANNING HISTORY

There have been a number of planning applications on this site as detailed below:

Reference	Proposal description	Decision and date
FUL/2020/0013	Demolition of existing garages and development of x2 4-bed houses	REFUSED 23/03/2020
DEM/2020/0125	Application for Prior Notification of proposed demolition of garages	Details required
R/2009/0599	Erection of a detached bungalow	REFUSED 2/07/2009
R/2008/1568	Erection of two semi-detached houses	REFUSED 15/10/2008
S/1966/1485	Retention and continued use of lock up garage	APPROVED 15/07/1966
S/1960/0892	Erection of lock up garage	APPROVED 30/09/1960

The previous application FUL/2020/0013 was refused on the grounds that:

1. The proposed development would be contrary to Policy DE1 and H3 of the Coventry Local Plan 2016 and the objectives of the National Planning Policy Framework 2019 by reason of:

i) its siting, design and materials would disrupt the rhythm of development and be out of keeping with the established street scene and contrary to the grain of development in the area to the detriment of the visual amenities and general character of the locality.

ii) the siting, design and massing in close proximity to neighbouring residential properties which would result in increased overlooking, unacceptable visual

intrusion and an overbearing impact on the living conditions of occupiers of No's. 294 Glendale Road and No's 58 and 60 Overdale Road;

2. Insufficient information has been submitted to demonstrate that the loss of garages and parking spaces will not result in the intensification of demand for on street parking in the locality in the absence of on-site parking provision. It is considered the proposal would result in an increase of demand for on-street parking in the locality, which would be to the detriment of highway safety, the free flow of traffic and to the amenities of the occupiers of adjacent dwellings. The proposal is contrary to Policies AC2 and AC3 of the Coventry Local Plan 2016, in particular 'the Car and Cycle Parking for New Development (Appendix 5)' and the aims and objectives of the NPPF 2019.

Prior to that both applications in 2008 and 2009 were refused on the grounds of the loss of well used parking in an area that was considered at that time to have a shortfall in available on street parking. Application R/2008/1568 was also refused on grounds that the siting, design and massing of the two storey dwellings in relation to Nos 294 Glendale Road and No.s 58 and 60 Overdale Road would result in increased overlooking, unacceptable visual intrusion and an overbearing impact on living conditions.

POLICY

National Policy Guidance

The National Planning Policy Framework (NPPF) sets out the Government's planning policies for England and how these are expected to be applied. It sets out the Government's requirements for the planning system only to the extent that is relevant, proportionate and necessary to do so, and identifies that the purpose of planning is to contribute to the achievement of sustainable development. Achieving sustainable development means that the planning system has three overarching objectives: economic, social and environmental objective which are interdependent and need to be pursued in mutually supportive ways. Of specific relevance to key issues is Part 2: Achieving Sustainable Development; Part 11: Making Efficient Use of Land; and Part 12: Achieving Well Designed Places.

The National Planning Practice Guidance (NPPG) adds further context to the NPPF and it is intended that the two documents are read together.

Local Policy Guidance

The current local policy is provided within the Coventry Local Plan 2016, which was adopted by Coventry City Council on 6th December 2017. Relevant policy relating to this application is:

Policy DE1: Ensuring High Quality of Design
Policy EM2: Building Standards
Policy EM6: Redevelopment of previously developed land
Policy AC1: Accessible Transport Network
Policy AC2: Road Network
Policy AC3: Demand Management
Policy AC4: Walking and Cycling
Policy DS1: Overall Development Needs
Policy DS3: Sustainable development
Policy H3: Provision of New Housing

Policy H4: Securing a Mix of Housing
Policy H9: Residential Density
Policy GE4: Tree Protection
Policy EM1: Planning for Climate Change Adaptation
Policy EM2: Building Standards
Policy EM5: Sustainable Drainage Systems (SuDS)
Policy EM6: Redevelopment of Previously Developed Land
Policy EM7: Air Quality

Supplementary Planning Guidance/ Documents (SPG/ SPD):

SPG Design Guidance for New Residential Development
SPD Delivering a More Sustainable City
SPD Coventry Connected –
- Appendix 5-car park and cycle parking standards
SPD Air Quality 2019
SPD Trees and Developer Guidance October 2020
SPG Design Guidance for Extending Your Home

CONSULTATION

No objections subject to conditions have been received from:

- Highways - (07/04/2021)
- Environmental Protection - (30/11/2020)
- Ecology - (11/12/2020)

At the time of writing the report comments have not been received from:

- Waste Services.

Immediate neighbours and local councillors have been notified; a site notice was posted on 02/12/2020. Further to an amended Parking Survey, further notification was carried out on 05/03/2021. Additional comments received on the amended parking survey have been included in the summary below.

15 letters of objection have been received, several which included photographs of parking in the area, raising the following material planning considerations:

- a) Loss of off-street parking, loading and unloading that the site provides to residents who don't have off-street parking, visitors and the nearby Whoberley Hall school, and emergency and care workers
- b) Increase in distance for residents (majority of whom are older and/or have mobility issues) to their homes from alternative on street parking
- c) The times of the original parking survey does not reflect the actual situation
- d) The timing of the updated parking survey during lockdown
- e) Increased capacity for road traffic accidents
- f) Loss of storage space
- g) Increase in on street parking and 'pavement parking'
- h) Pitched roofs in an area where bungalows have flat roofs would be unsympathetic
- i) Loss of daylight and sunlight, and overshadowing to adjacent gardens and properties
- j) Loss of privacy and outlook

- k) Overbearing development
- l) Overdevelopment 'cramming'
- m) Disruption and impact on health from noise, vibration and dust during construction
- n) Concerns about the number of applications that have been submitted to date on this site despite being refused have been raised.

Within the letters received the following non-material planning considerations were raised, these cannot be given due consideration in the planning process:

- o) impact on property values
- p) and access and maintenance of adjacent property 294.

Any further comments received will be reported within late representations.

Cllr Bally Singh objects as follows: The pitched roof of the proposed bungalows will be detrimental to the open views of neighbours, and the loss of parking / garage / access space for residents impacted. I would welcome designs that encouraged a sustainable flat roof if this application is approved, and conditions that existing parking and access is not hindered.

APPRAISAL

The main issues in determining this application are:

- The principle of development
- Loss of parking
- Highways and access considerations
- Design and visual considerations
- Residential amenity
- Ecology and biodiversity
- Air quality and ground conditions

Principle of development

When considering the suitability of a site for housing development that is not already allocated, Policy H3 of the Local Plan must be considered to ensure it is situated within a sustainable location and will ensure the creation of an appropriate and acceptable residential environment. Directly relevant to the principle of the development is Policy H3 (4) which requires development to be in a sustainable location, wherever possible within:

- a) within 2km radius of local medical services;
- b) within 1.5km of a designated centre within the city hierarchy (policy R3);
- c) within 1km radius of a primary school;
- d) within 1km of indoor and outdoor sports facilities; and
- e) within 400m of a bus stop
- f) within 400m of publicly accessible green space

The location is sustainable being accessible to all the above having access to services and facilities, within c. 300 m of Winsford Avenue Local Centre, and the medical centre on Whitaker Road being within c. 400m of the site. Bus services operate along Lyndale Road within 400 m to the south of the site. The location satisfies policy H3(4) of the Local Plan.

Paragraph 68 of the NPPF supports small sized sites for residential development, recognising the contribution that suitable windfall sites can make to the housing requirement within existing settlements.

Paragraph 118 (c) of the NPPF requires that substantial weight is given to the value of using suitable brownfield sites within settlements for homes and other identified needs, and part (d) promotes and supports the development of under-utilised land and buildings.

The principle of the residential development of this brownfield site to deliver additional housing provision is therefore supported in accordance with Policy H3, subject to the proposal satisfying the other relevant parts of policy H3, specifically part 1 – high quality design and part 3 – providing a suitable residential environment. These relate to matters of detail and are discussed under the relevant headings below. Part 5 also requires proposals to conform with all other relevant plan policies. This is discussed further in the relevant sections below.

Loss of Parking

Delivering a suitable residential environment includes ensuring safe and appropriate access as set out in Policy H3 (part 3) of the Local Plan. The loss of parking is a key part of the assessment of this application as there would be a direct loss of parking provision as a result of the proposal, and displaced parking can give rise to issues relating to both residential amenity and highway safety if there is no capacity within the area to absorb the displacements.

Representations received, including photographs, indicate that there are existing parking problems in Overdale Road at present, particularly worse during school drop off and pick up times, where cars are parked partly on the pavement, and that the site is relied on by residents and others. During officer visits to the site, 5 cars (29/4/2021 at 11.30am) and 7 cars (2/12/2020 at 2.30pm) were parked in the spaces.

It is noted that the previous application FUL/2020/0013 was refused on the grounds that insufficient evidence had been submitted to demonstrate that the loss of garages and parking spaces will not result in the intensification of demand for on street parking in the locality in the absence of on-site parking provision. The garage analysis and parking review that was submitted as part of that application did not include the parking provision on the additional hardstanding area within the site (13 spaces), and only included the garages. This application has included an assessment of the garages and parking on the site, within the Highways Statement, which includes a parking survey.

In their initial comments on the application (dated 8/12/2020) an objection was received from the Highways Authority, in which they commented that *“as part of the application the applicant has failed to provide evidence demonstrating that the development is not contrary to the Local Plan policy and should accord with Car and Cycle Parking Standards within Appendix 5 Coventry Local Plan 2016”*.

Further to discussion with the applicant’s agent, it became evident that since the previous refusal and the submission of this application, that the parking beat guidance had been updated, hence the original survey did not meet the most up to date guidance. An amended Highways Statement with parking beat survey was received on 1/3/2021.

The single beat parking survey was undertaken on 10th February 2021 between 00:30 and 05:30 on all streets within 200m of the proposed development in accordance with Coventry City Council’s Parking Beat Survey Standards for Residential Developments. Whilst parking surveys would not ordinarily be completed during

school holidays or in the weeks immediately either side of those dates, the applicant's agent confirmed with the Highways Authority that the parking survey could be carried out at this time due to the lockdown situation which was preventing holidays etc.

Notwithstanding that the size of the garages (internal dimensions on average 4.8 m x 2.35 m) is below that recommended in the Coventry Connected SPD of 3 m x 6 m internally, 3 of the 4 let garages have been included in the parking beat survey (1 of the 4 let garages is let to a person living c. 3km from the site), hence it was not included. The updated Highways Statement states that the results show that the spare capacity on the roads within 200m of the site would allow 159 cars to park on-street, and that Overdale Road itself had spare capacity for 51 - 53 cars.

The survey concludes that *"only 3 garages and 13 parking bays, so a total of 16 parking spaces are likely to be displaced locally as part of the proposals and will be fully accommodated by spare capacity on Overdale Road."* It continues that *"it also demonstrates that there is further capacity on Overdale Road (or other streets within 200m from the site), to comfortably accommodate parking associated with the 4 garages that are either already out of use and or currently rented to someone outside of the local area, or to allow for modest peaks and troughs in parking demand that may be reasonably expected."*

It is not disputed that the parking provision on the site is well used at times, as evidenced by third party representations and site visits by your officer. However, sufficient evidence has been submitted in the Highways Statement to demonstrate that there is sufficient space within Overdale Road and the wider area surveyed to absorb displaced parking, from the 13 car parking spaces and the 4 let garages. Whilst a level of inconvenience may arise for residents who utilise the car parking spaces, as a result of having to park further from their home, no significant harm would arise from the loss of the spaces that would result in detriment to highway safety and the residential amenity of local residents such that reason for refusal No. 2 on the previous application has been addressed. Additionally, enough parking allocation has been provided for the proposed dwellings to ensure that no displacement is experienced as a result of the proposal.

The proposal would therefore accord with Policies DE1 and HE3 of the Coventry Local Plan 2016 and the aims and objectives of the National Planning Policy Framework in respect of safe and appropriate access due to the capacity of nearby roads to absorb any displaced parking.

Highways and access

Policy AC3 of the Local Plan acknowledges that the provision of car parking can influence occurrences of inappropriate on-street parking which can block access routes for emergency, refuse and delivery vehicles, block footways preventing access for pedestrians, reduce visibility at junctions and impact negatively on the street scene. Proposals for the provision of car parking associated with new development are assessed on the basis of parking standards set out in Coventry Connected SPD. The car parking standards also include requirements for the provision of electric car charging and cycle parking infrastructure.

The proposal would provide for 4 car parking spaces to serve the two dwellings. The existing access would remain to serve the two dwellings. This would accord with the SPD parking requirements. Pedestrian visibility splays of 2 m x 2 m can be provided and are shown on the proposed site plan to both sides of the access. These areas should be maintained free of any obstructions above a height of 600mm and can be conditioned.

Further to the receipt of the updated Highways Statement, including parking survey and site plan showing the required pedestrian visibility splays, the Highways Authority raise no objection to the proposal subject to conditions requiring a Construction Method Statement, laying out of the access, turning and parking areas as shown on the site plan and the provision and retention of unobstructed visibility splays. Additionally, provision should be made for cycle storage for each dwelling which can be conditioned.

The proposal would accord with Policies AC1, AC2, AC3, AC4, DE1 and H3 of the Coventry Local Plan 2016, Coventry Connected SPD, and the aims and objectives of the National Planning Policy Framework such that the access, layout and parking provision would not be detrimental to highway safety and sufficient onsite vehicle and cycle parking can be provided.

Design and Visual considerations

Policy DE1 of the Local Plan seeks to ensure high quality design and development proposals must respect and enhance their surroundings and positively contribute towards the local identity and character of an area, having regard to scale, layout, density, mass, design, materials and architectural features. Policy H3 (part 1) of the Local Plan requires that new residential development must provide a high-quality residential environment which assists in delivering urban regeneration or contributes to creating sustainable communities and which overall enhances the built environment.

Paragraph 124 of the NPPF identifies that the creation of high-quality buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.

Notwithstanding the description of the garages in the Planning Statement, they appear to be in good condition, although having no architectural or historical merit. Their demolition would not result in the loss of a high-quality structure or heritage asset.

Concerns have been raised that the proposal would result in over-development of the site. The density proposed equates to 40 dwellings per hectare (dph) which would accord with Policy H9 of the Local Plan that identifies that densities of a minimum 35 dwellings per hectare (dph) should be provided on previously developed land, and is such that it would be appropriate in the context of the established pattern of development in the vicinity of the site.

The previous application was refused on the grounds of the siting, design and materials disrupting the rhythm of development and being out of keeping with the established street scene. The proposal has been amended to provide bungalows. These would better reflect the pattern of development to the north and east of the site, where there are flat roofed bungalows. To the south of the site are two storey pitched roof dwellings. The pitched roof bungalows proposed would complement the pattern of development and be an appropriate transition between the existing flat roof bungalows and two storey pitched roof dwellings, as shown on the proposed street scene.

This site is a candidate for modular construction as one of the number of sites where this build approach is being taken by Citizen on a number of former garage sites. The materials palette has been amended to better reflect that of the surrounding properties with brick slips and concrete roof tiles. Details of the texture and colour finish of these are matters that can be conditioned.

Planting is proposed around the frontage parking. The existing wall set back from Overdale Road is to be removed which would allow for proposed planting to integrate

with the existing. A 1.8 m high brick boundary wall would enclose the side of plot 1 to the adjacent footpath.

The development siting, design and scale would respond to its surroundings and the external finish, landscaping and boundary materials can be secured by condition, such that the proposal would accord with the principles of good design in policies DE1, H3 and H9 of the Local Plan, SPG Design Guidance for New Residential Development and part 12 of the NPPF and National Design Guide.

Residential amenity

Adequate amenity space is required in Policy H3 of the Local Plan and the design Guidance for new residential development identifies space requirements for dwellings. Both dwellings would have in excess of the minimum 30 sq m of rear external amenity space, which would be suitably enclosed to provide privacy and would not be overlooked.

Turning attention to the amenity of neighbouring residents, the previous application was refused on the grounds that the siting, design and massing in close proximity to neighbouring residential properties would result in increased overlooking, unacceptable visual intrusion and an overbearing impact on the living conditions of occupiers of No's. 294 Glendale Road and No's 58 and 60 Overdale Road.

The neighbouring property to the east, the flank elevation of No. 294 Greendale is on the eastern boundary of the site. The previous concern with regard to the 12 m distance from the proposed dwelling to the side of 294 Greendale has been addressed as there would be no potential for overlooking of the garden of no. 294 from the proposed bungalows or an overbearing form of development due to their single storey nature. Permitted development rights could also be removed to prevent future additions at roof level that may give rise to a loss of privacy.

The Design Guidance for new residential development SPG identifies that a minimum of 10 m will normally be required in the interests of achieving acceptable standards of daylight, to minimise overshadowing and maintain privacy. The proposal would achieve c.8.2 m garden depth to the flank elevation of No. 294. Deviation from the 10 m depth in this case is considered acceptable here as the bungalows would not present any privacy issues or other amenity issues in relation to No. 294.

Particular care should be taken with a proposal that would obstruct the southerly aspects of an adjoining property or where the application sites ground level is higher than adjoining properties. Both apply in this case and have been carefully considered, particularly as there is a change in the levels (c. 1.8 m) adjacent to the site, from north to south.

Neighbouring properties to the north Nos. 56, 58 and 60 Overdale Road are bungalows. Previous concerns related to the massing/height of the proposed dwellings and level changes, such that the proposed dwellings would have a detrimental impact upon the amenity of these occupants regarding visual intrusion and overbearing impact. The houses previously refused had a height to eaves of 5.02 m and a ridge height of 9.02 m. The proposed dwellings have an eaves height of 2.56 m and a ridge height of 5.93m. The dwellings are located on the same alignment as previously submitted, that is north/south with the side elevation and gable end facing the rear of no. 58 Overdale Road at a distance of 14 m.

The adjacent properties being to the south of the site will experience some increased overshadowing compared to that currently experienced from the adjacent garages and tree. However, due to the north/south orientation of the bungalows, compared with the

elongated east /west orientation of the garages, any overshadowing experienced is not considered harmful such that it would significantly reduce light to the rear of the adjacent properties, through excessive overshadowing.

There are no openings to habitable rooms proposed to the side elevation, and with the intervening boundary treatments, and the single storey nature of the proposed development there would be no loss of amenity through overlooking or an overbearing sense of enclosure.

The residential use is compatible with surrounding residential uses and the scheme is not considered to create any significant impact upon the amenities of the occupiers of surrounding properties in terms of outlook, light or privacy.

Third parties have raised concerns regarding these matters, but the development of the site to provide bungalows would not give rise to a significant loss of residential amenity for the reasons above.

Whilst there would be some disruption to residential amenity during construction this is to be expected during the construction phase and would be temporary in nature. A construction management plan is required by condition which will allow the Local Planning Authority to consider matters relating hours of work, measures to control the emission of dust and dirt during demolition and construction, measures to minimise noise disturbance to neighbouring properties during demolition and construction and measures to control the presence of any asbestos. These controls are considered necessary in the interests of residential amenity.

The existing parking pressures and the level of use of the site for parking raised by third parties is noted. As detailed in the loss of parking section above, it has been demonstrated that the loss of parking provision on the site can be absorbed in the surrounding area, such that there would be no adverse impact on the living conditions of adjacent and nearby residents, through noise and disturbance associated with vehicle movements and displacement of existing parking.

The proposal has satisfactorily addressed the previous reason for refusal in respect of residential amenity and would therefore accord with policies DE1 and H3 of the Local Plan 2016, and paragraph 127(f) of the NPPF.

Ecology and biodiversity

The proposal would not have any impact on any statutory or non-statutory wildlife sites. There are no records of any notable wildlife from the site, there are nearby records of hedgehogs and bats will be present. The existing site consists of garages and hardstanding with limited biodiversity value. The likelihood of bat roosts within the garages is very low. The creation of gardens to replace the existing hardstanding will provide a small biodiversity gain.

The application includes a Tree Survey (CGO Ecology, June 2020) which states that there are no trees within the application footprint. T2 adjacent to the north western corner of the site is prominent in the street scene and its retention is important to the visual amenity of the public realm.

An informative in respect of protected species has been recommended by the Council's Ecologist. A condition to require a Tree Protection Plan and Arboricultural Method Statement as recommended in the Tree Survey report can be imposed. The proposal would therefore accord with policies GE3 and GE4 of the Local Plan, and the Trees and Developer Guidance SPD.

Air Quality and Ground conditions

Air Quality

With the city being declared an Air Quality Management Area since 2009 for nitrogen dioxide, primarily as a result of traffic related emissions, and the more recent Ministerial Direction that requires the city council to implement a package of measures to reduce nitrogen dioxide emissions to legal levels within the 'shortest possible time', Policy AC1 of the Local Plan and Coventry Connected SPD requires the infrastructure for electric vehicles to be installed through planning condition; in this case one space per dwelling. The policies reflect paragraph 181 of the NPPF.

This can be secured through a condition requiring electric vehicle charging points to be provided, such that the proposal would accord with Policies AC1 and EM7 of the Local Plan, Coventry Connected SPD, Air Quality SPD and the objectives of the NPPF.

Ground Conditions

Policy EM6 seeks to ensure that redevelopment of previously developed land does not have a negative impact on water quality, either directly through pollution of surface or ground water or indirectly through the treatment of wastewater by whatever means. Environmental Protection raise no objection subject to conditions in relation to a contaminated land (watching brief) and details of any imported soils, including chemical sampling demonstrating their suitability for use.

Equality Implications

Section 149 of the Equality Act 2010 created the public sector equality duty. Section 149 states:-

(1) A public authority must, in the exercise of its functions, have due regard to the need to:

- a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
- a) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- b) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

Officers have taken this into account and given due regard to this statutory duty, and the matters specified in Section 149 of the Equality Act 2010 in the determination of this application.

There are no known equality implications arising directly from this development.

Conclusion

The proposed development is acceptable in principle. The loss of parking would not result in the unacceptable displacement of parking, and there would be no significant harmful impacts on residential amenity, highway safety, ecology and biodiversity for the reasons set out above. The reason for this recommendation for approval subject to the conditions set out below, is that the proposal is in accordance with Policies DS3, DE1, H3, H9, GE3, GE4, AC1, AC2, AC3, AC4, EM6 and EM7 of the Coventry Local Plan 2016, adopted supplementary planning documents and guidance, together with the aims of the NPPF.

CONDITIONS:\REASONS

1. The development hereby permitted shall begin not later than 3 years from the date of this decision.

Reason: *To conform with Section 91 of the Town and Country Planning Act 1990 (as amended)*

2. The development hereby permitted shall be carried out in accordance with the following approved plans and documents: 100-407AR/001 Rev G Site Layout; 100-407AR/002C Site Location; 100-407AR/003 Rev E Boundary Treatment; 100-407AR/007 - Type B- 2B3P; 100-407AR/008A Street Scene; 25080_06_170_218 Topographical Survey; 25080_06_200_218 Underground detection survey; Existing site layout 1:500; Planning Statement 100-407 Rev C dated June 2020; Highway Statement reference CGSW407-BSP-ZZ-XX-RP-D-0001-P03 dated 26 February 2021; Tree Survey, Arboricultural Impact Assessment and Tree Constraints Plan version 2 dated 12 June 2020; Phase II Geo-Environmental Investigation report ref 25080-GI-23-01 dated December 2019 and Ownership certificate A Justification Statement.

Reason: *For the avoidance of doubt and in the interests of proper planning.*

3. Prior to their incorporation into the development hereby permitted, sample details of all facing and roofing materials shall be submitted to and approved in writing by the Local Planning Authority. These details shall be installed only in full accordance with the approved details prior to the first occupation of the development and thereafter shall be retained and shall not be removed or altered in any way.

Reason: *To ensure that the proposed development has a satisfactory external appearance in the interests of the visual amenities of the area in accordance with Policy DE1 of the Coventry Local Plan 2016.*

4. Prior to the first occupation of the development hereby permitted, details of both hard and soft landscaping works shall be submitted to and approved in writing by the Local Planning Authority. Details of hard landscaping works shall include boundary treatments, including full details of the proposed boundary walls, railings and gates to be erected, specifying the type of bricks and colour of the railings and gates; footpaths; and hard surfacing (which shall be made of porous materials or provision shall be made to direct run-off water from the hard surface to a permeable or porous area). The hard landscaping works shall be completed in strict accordance with the approved details within three months of the first occupation of any of the dwellings hereby permitted; and all planting shall be carried out in accordance with the approved details within the first planting and seeding seasons following the first occupation. Any tree(s) or shrub(s) which within a period of five years from the completion of the development dies, is removed or becomes, in the opinion of the Local Planning Authority; seriously damaged, defective or diseased shall be replaced in the next planting season with another of similar size and species. All hedging, tree(s) and shrub(s) shall be planted in accordance with British Standard BS 8545:2014 Trees: from nursery to independence in the landscape - Recommendations and BS4428 - Code of Practice for General Landscape Operations.

Reason: *To ensure a satisfactory standard of appearance of the development in the interests of the visual amenities of the area in accordance with Policies GE1 and DE1 of the Coventry Local Plan 2016.*

5. Prior to the first occupation of the development hereby permitted, a landscape management plan, including long term design objectives, long term management responsibilities and maintenance schedules for all landscape areas (other than domestic gardens within the curtilage of a single dwellinghouse), shall be submitted to and approved in writing by the Local Planning Authority. The landscape management plan shall be implemented as soon as the approved landscaping is carried out and shall not be withdrawn or altered in any way.

Reason: *To ensure a satisfactory standard of appearance over the lifetime of the development in the interests of the visual amenities of the area in accordance with Policy GE1 and DE1 of the Coventry Local Plan 2016.*

6. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended), or any statutory instrument amending, revoking and/or replacing that Order, no buildings or enclosures shall be provided within the curtilage of, and no extensions, enlargements or additions erected or constructed, or windows or other form of opening be provided in the rear roof slopes to the dwelling houses hereby permitted, without the prior grant of planning permission by the Local Planning Authority.

Reason: *Having regard to the layout and general nature of the proposed development and its relationship with adjoining properties it is important to ensure that no development is carried out which would detract from the appearance of the area and affect the amenities of adjacent properties in accordance with Policies H3 and DE1 of the Coventry Local Plan 2016.*

7. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended), or any statutory instrument amending, revoking and/or replacing that Order, no gate, fence, wall or other means of enclosure, other than shown on the approved plans, shall be erected, constructed or improved or altered such as to place the structure in front of the forwardmost part of any dwelling houses fronting a highway or footpath without the prior grant of planning permission by the Local Planning Authority.

Reason: *Having regard to the open plan layout and general nature of the proposed development it is important to ensure that no development is carried out except with the permission of the Local Planning Authority, which would detract from the appearance of the area and affect the amenities of adjacent properties in accordance with Policies H3 and DE1 of the Coventry Local Plan 2016.*

8. No development or other operations (including any demolition, site clearance or other preparatory works) shall commence unless and until the tree protection measures identified in the approved application Arboricultural Impact Assessment and Tree Constraints Plan version 2 dated 12 June 2020 have been put into place in strict accordance with the approved details and thereafter they shall remain in place during all construction work. These measures shall be detailed in a Tree Protection Plan (TPP) and Arboricultural Method Statement (AMS) in accordance with the recommendations in the approved documentation and submitted to and approved in writing by the Local Planning Authority. In addition no excavations, site works, stock piling, trenches or channels shall be cut or pipes or services laid, (other than works necessary to facilitate the construction of the dwellings and landscaping of the site), and no

fires shall be lit within 10 metres of the nearest point of the canopy or root protection area of any protected tree(s); no equipment, machinery or structure shall be located within this zone; no mixing of cement or use of other contaminating materials or substances shall take place within, or close enough to, a root protection area that seepage or displacement could cause them to enter a root protection area or any other works carried out in such a way as to cause damage or injury to the tree(s) by interference with their root structure and that no soil or waste shall be deposited on the land in such a position as to be likely to cause damage or injury to the tree(s).

Reason: *To protect those trees which are of significant amenity value to the area and which would provide an enhanced standard of appearance to the development in accordance with Policy GE3 and GE4 of the Coventry Local Plan 2016.*

9. Prior to the first use of the access in connection with the dwellings hereby permitted, the 2 metre by 2 metre visibility splays as shown on the approved plans shall be provided and thereafter retained free from obstruction at all times above a height of 600mm.

Reason: *In the interests of highway safety in accordance with the aims and objectives of Policies AC1 and AC2 of the Coventry Local Plan 2016.*

10. Prior to occupation of any of the dwellings hereby permitted, details of cycle parking provision to provide at least 1 cycle parking space per dwelling, shall be submitted to and approved in writing by the Local Planning Authority. The cycle parking provision shall be provided in full accordance with the approved details prior to first occupation of the dwellings and thereafter those facilities shall remain available for use at all times and shall not be removed or altered in any way.

Reason: *In the interests of encouraging the use of alternative modes of transport with the aim of creating a more sustainable city in accordance with Policies DS3, AC3 and AC4 of the Coventry Local Plan 2016.*

11. The development hereby permitted shall not be occupied unless and until the access to the site, turning areas and car parking areas shown on the approved plan have been laid out and properly consolidated, delineated, surfaced, drained, free of loose stone and otherwise constructed in accordance with details to be submitted to and approved in writing by the Local Planning Authority. These areas shall thereafter be retained and kept available for those uses at all times.

Reason: *In the interests of highway safety and to ensure the free flow of traffic in accordance with the aims and objectives of Policies AC1, AC2 and AC3 of the Coventry Local Plan 2016.*

12. No development (including any demolition) shall take place unless and until a Construction Management Plan (CMP) has been submitted to and approved in writing by the Local Planning Authority. The CMP shall include details of:
 - hours of work;
 - hours of deliveries to the site;
 - the parking of vehicles of site operatives and visitors during the demolition/construction phase;
 - the delivery access point;
 - the loading and unloading of plant and materials;

- anticipated size and frequency of vehicles moving to/from the site;
- the storage of plant and materials used in constructing the development;
- the erection and maintenance of a security hoarding including decorative displays and facilities for public viewing where appropriate;
- wheel washing facilities and other measures to ensure that any vehicle, plant or equipment leaving the application site does not carry mud or deposit other materials onto the public highway;
- measures to control the emission of dust and dirt during demolition and construction;
- measures to control the presence of asbestos;
- measures to minimise noise disturbance to neighbouring properties during demolition and construction;
- details of any piling together with details of how any associated vibration will be monitored and controlled; and
- a scheme for recycling / disposing of waste resulting from demolition and construction works.

Thereafter, the approved details within the CMP shall be strictly adhered to throughout the construction period and shall not be amended in any way.

Reason: *The agreement of a Construction Management Plan prior to the commencement of development is fundamental to ensure a satisfactory level of environmental protection; to minimise disturbance to local residents and in the interests of highway safety during the construction process in accordance with Policies EM7, AC1 and AC2 of the Coventry Local Plan 2016.*

13. In the event that contamination is found at any time when carrying out the approved development, that was not previously identified, it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken, and where remediation is necessary a remediation scheme must be prepared which shall be submitted to and approved in writing by the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

14. Prior to the first occupation of any of the dwellings hereby permitted, details of any imported soils, including chemical sampling demonstrating their suitability for use shall be submitted to and approved in writing by the Local Planning Authority.

Reason: *To safeguard health, safety and the environment in accordance with Policy EM6 of the Coventry Local Plan 2016 and the aims and objectives of the NPPF.*

15. Any gas boilers installed on site shall have a dry NOx emission rate of no more than 40mg/kWh. One electric vehicle recharging point per dwelling shall be provided prior to occupation in accordance with details to be submitted to and approved in writing by the Local Planning Authority, and which shall include the siting and design of the recharging points. Recharging points shall be installed in accordance with the approved details and shall not be removed or altered in any way and shall be kept available for such use by residents at all times.

Reason: *To mitigate the impacts of development on air quality in accordance with Policy DS3 of the Coventry Local Plan 2016 and the aims and the objectives of the NPPF.*

16. The dwellings hereby permitted shall not be occupied unless and until provision has been made for the siting of bins within the side/rear gardens as shown on the approved plans. All bins must be stored within the rear/side gardens and not positioned on the public highway or in the open, unless on bin collection days.

Reason: *In the interests of the amenities of the future occupants of the residential accommodation in accordance with Policy DE1 of the Coventry Local Plan 2016.*



REFEREMNCE NUMBER
FUL/2020/1334

SITE:
GARAGES OVERDALE ROAD

SCALE 1:1250





Revision:	Date:	Drawn:	Check:
A	Footprints amended	28.05.19	YM
B	Red line amended to include entire proposed development area & drawing status updated to planning.	31.05.19 / 04/06/19	GW / JR / CW
C	House type amended to Type G - 4B8P.	17.07.19	CW YM
D	House type amended to Type E - 5B8P. Schedule amended.	17.12.19	GW
E	House type amended to Type H - 4B6P. Schedule amended.	03.03.20	GDW JER
F	House type & schedule amended. Minor layout amendments. Bin storage added.	22.04.20	GDW JER
G	2m vis vsplays added to access	26.02.21	GDW JER



Project: A development at 31-37 Overdale Road, Coventry

Status: Planning

Client: Citizen Housing

Sheet title: Site Layout

Scale: 1:500 @ A4

Date: 17.04.19

Drawn: GW

Checked: JR

Ref: 100-407AR/001G

ACCOMMODATION SCHEDULE

100-407 31-37 Overdale Road

HOUSE TYPE	Beds	NUMBER	SQM	SQM Total	SQFT	SQFt total	%
B (Bungalow)	2b3p	2	62	124	667.37	1334.74	100.0%
Total		2		124		1334.74	100.0%

Total Site Area:	0.049		Hectares	
Total Site Area:	0.121		Acres	
Site Density:	40.98		DPH	

Approximate Tree Location

All drawing information is based on OS map data and is subject to change after receiving topographical survey and services information.



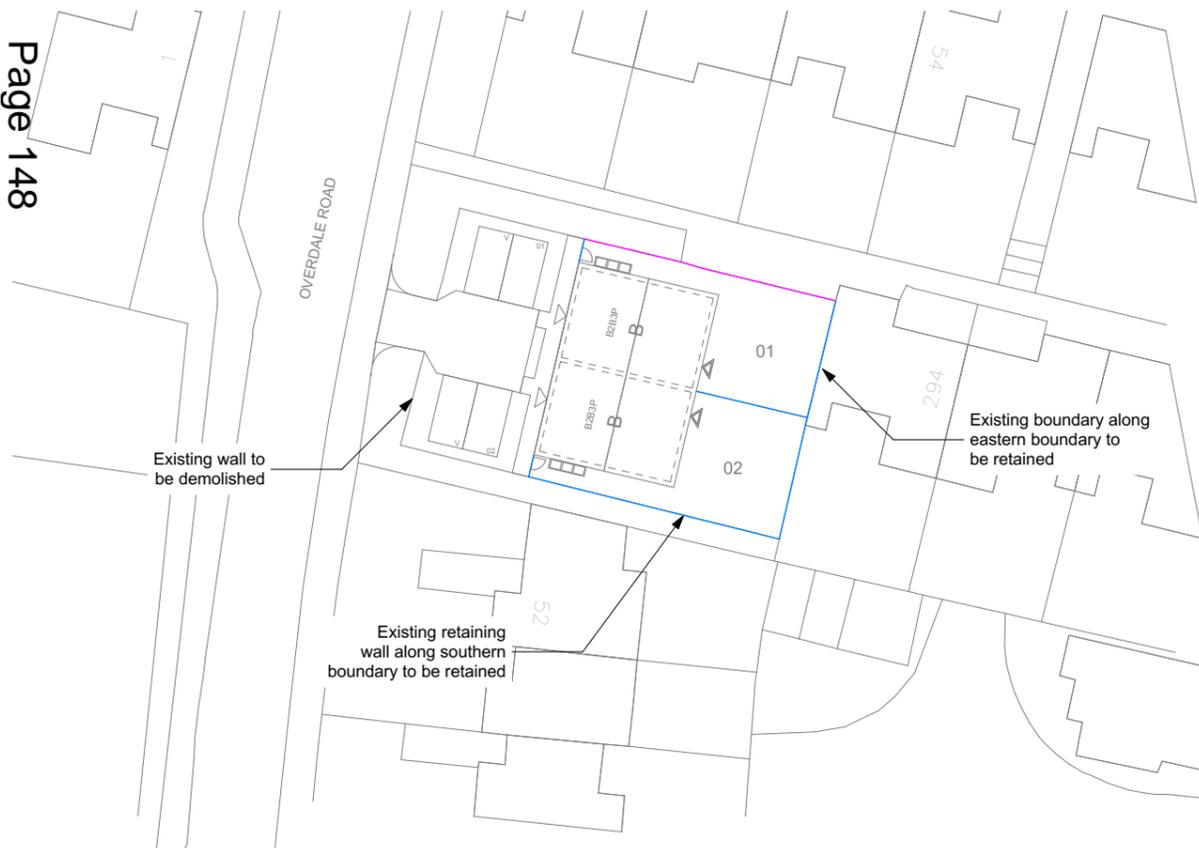
Revision:	Date:	Drawn:	Check:
A	Blue lines added.	26.06.19	CW JR
B	Map crop to consolidate land ownership discrepancies.	22.01.20	GW JR
C	Blue lines removed as per planning request.	28.01.20	GW



Project:	A development at 31-37 Overdale Road, Coventry
Status:	Planning
Client:	Citizen Housing
Sheet title:	Site Location
Scale:	1:1250@A4
Date:	25.05.19
Drawn:	GW
Checked:	JR

Ref: **100-407AR/002C**

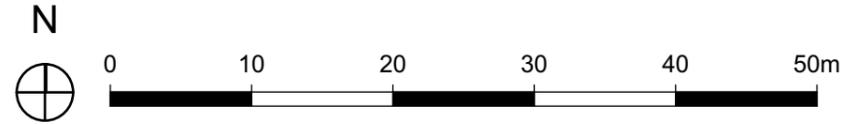
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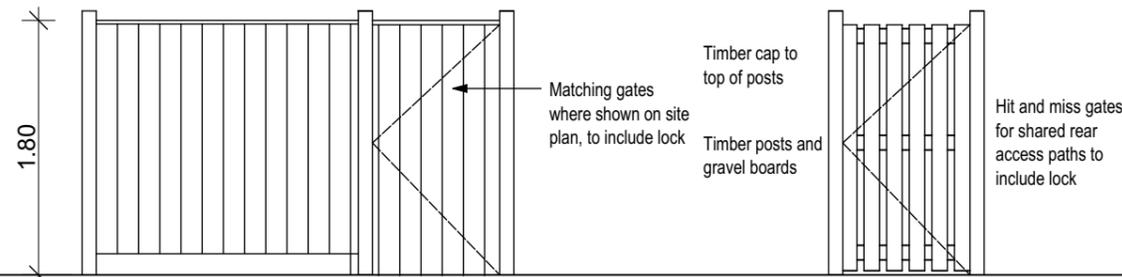
Revision:	Date:	Drawn:	Check:
A House type amended to Type G - 4B8P.	17.07.19	CW	YM
B Updated in accordance with revised site layout	20.12.19	GW	
C Updated in accordance with revised site layout	04.03.20	GDW	
D Updated in accordance with revised site layout. F3 boundary type added.	23.04.20	GDW	JER
E F3 boundary type amended to all brick. Existing wall & boundary photo/notation added.	01.03.21	GDW	JER

Boundary Type Key

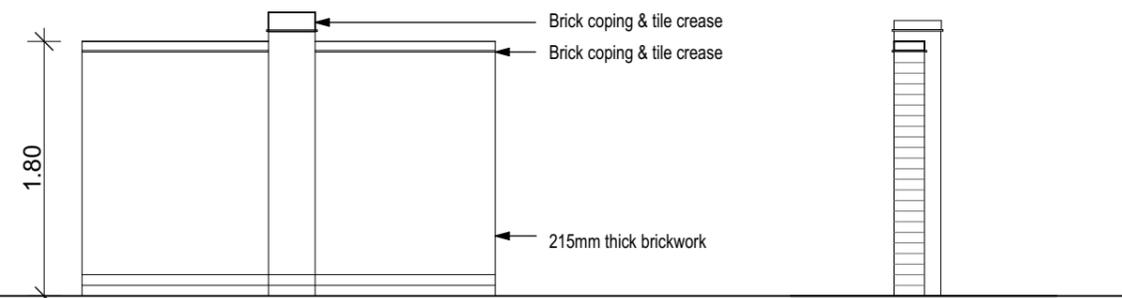
- F1 1800mm Close boarded fence/gates
- F3 1800mm brick wall



Boundary Treatment



F1 1800mm Close boarded fence/gates ———— Timber gate



F3 1800mm brick wall ————



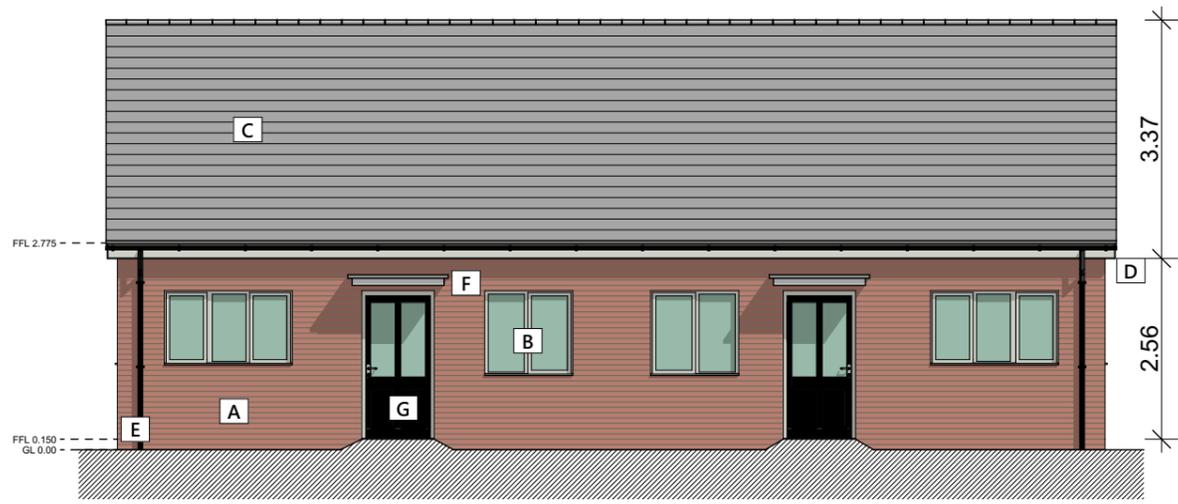
Project:	A development at 31-37 Overdale Road, Coventry
Status:	Planning
Client:	Citizen Housing
Sheet title:	Boundary treatment Plan
Scale:	1:500, 1:50 @ A4
Date:	25.05.19
Drawn:	GW
Checked:	JR
Ref:	100-407AR/003E



Existing boundary treatments - (A) brick and block wall to western boundary and (B) 1 metre block retaining wall to the southern boundary

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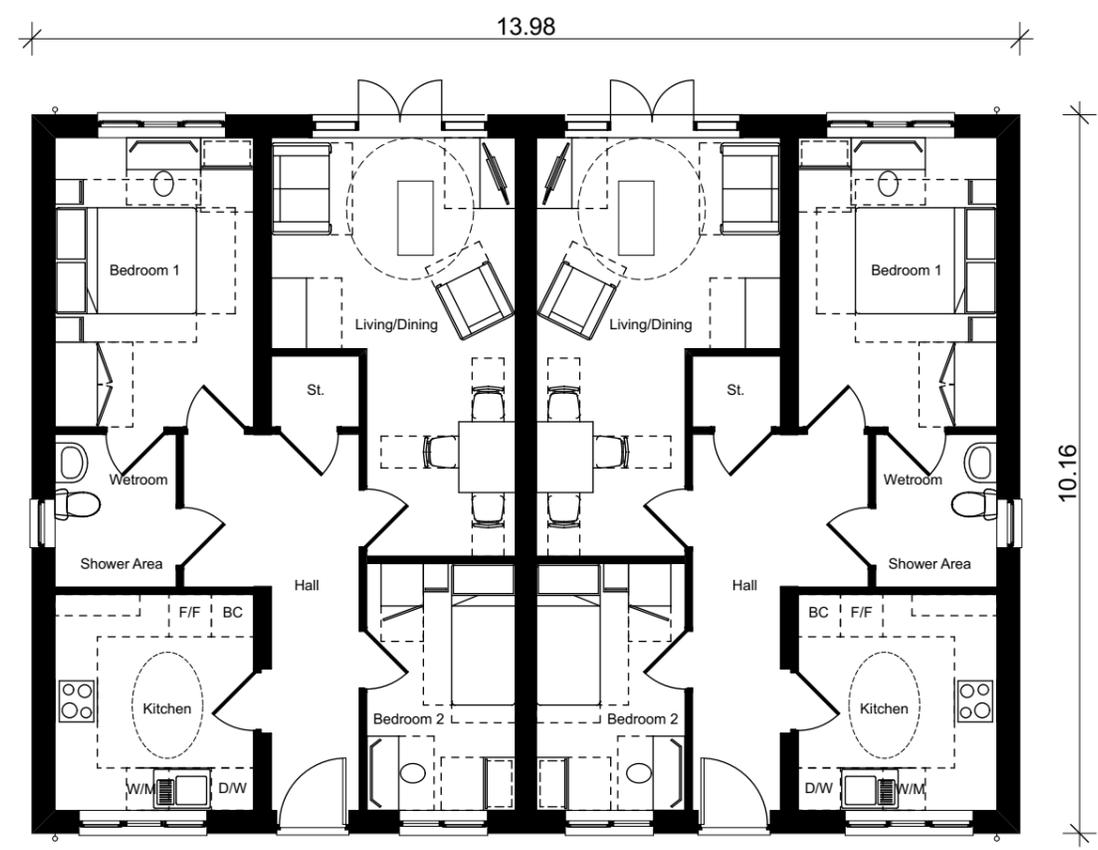
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Front Elevation



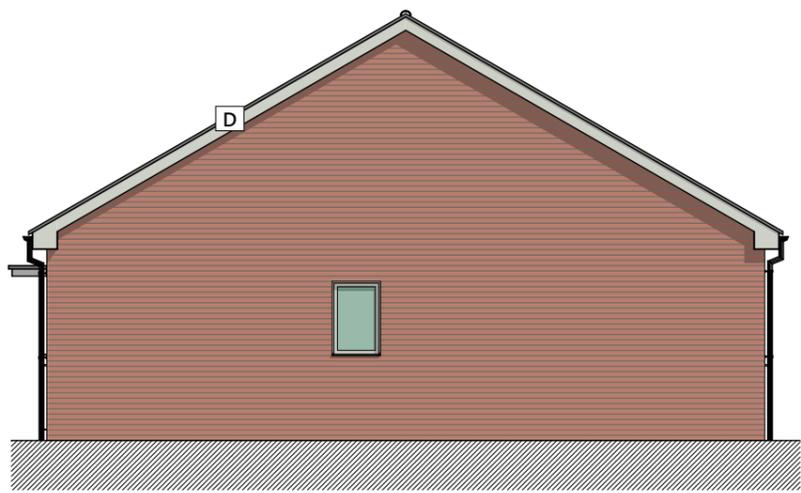
Rear Elevation



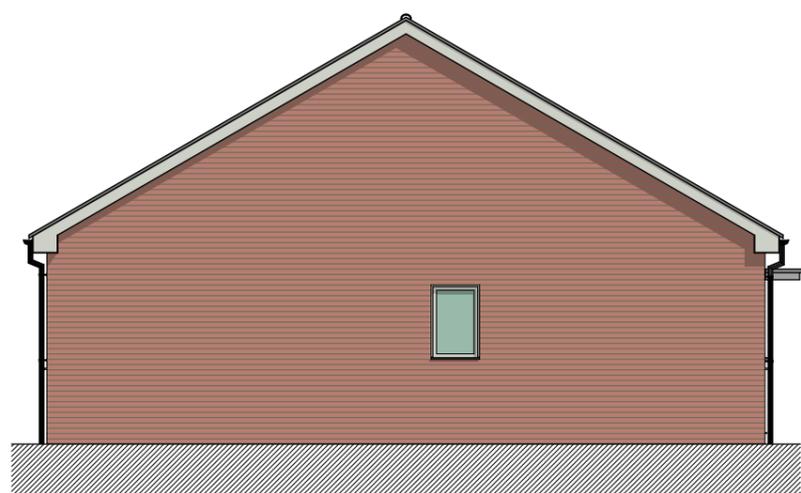
Ground Floor

Materials Key

- A. Brick slips (Colour to match existing development)
- B. White UPVC window frames (VEKA Infinity)
- C. Grey concrete Roof Tiles
- D. White UPVC Fascia & Barge Board
- E. Black UPVC downpipe/gutters
- F. Door canopy
- G. Black composite door (Sovereign Group)



Side Elevation



Side Elevation



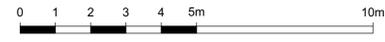
Project:	A development at 31-37 Overdale Road, Coventry
Status:	Planning
Client:	Citizen Housing
Sheet title:	Type B - 2B3P
Scale:	1:100@A3
Date:	03.03.2020
Drawn:	GDW
Checked:	JER
Ref:	100-407AR/007

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Street Scene A-A
Scale: 1:100@A2

Note: Levels indicative only and TBC upon further information /surveys if required.



Project:	A development at 31-37 Overdale Road, Coventry
Status:	Planning
Client:	Citizen Housing
Sheet title:	Street Scene
Scale:	1:100, 1:500 @A2
Date:	03.03.2020
Drawn:	GDW
Checked:	JER
Ref:	100-407AR/008A

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